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And last, but certainly not least...

**ALL THE MANY CITIZENS WHO HAVE VOLUNTERRED COUNTLESS
HOURS OVER MANY YEARS TO BRING THESE DESIGN STANDARDS TO
COMPLETION AND ADOPTION.
WE THANK YOU.**

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Chapter 1. Introduction & Background

1.1 Silverdale & the Regional Growth Center

The Silverdale Urban Growth Area (UGA), located in Central Kitsap County at the north end of Dyes Inlet, was established in 1998 with the adoption of the Kitsap County Comprehensive Plan. In 2006, as part of the adoption of the 10-year update to the Kitsap County Comprehensive Plan, the Silverdale UGA was expanded to encompass approximately 7,400 acres, the Silverdale Sub-Area Plan was adopted, and Draft Silverdale Design Guidelines were referenced in the Integrated Comprehensive Plan and Draft Environmental Impact Statement, Volume 3, Appendix B.

In 2003, the Kitsap Countywide Planning Policies and Puget Sound Regional Council (PSRC) recognized portions of the Silverdale UGA as a regional growth center for employment and population. Regional growth centers are intended to be dense and diverse urban centers with a regional focus on significant business, cultural, governmental, residential and recreational activities. Development should result in compact and centralized working, shopping and activity areas linked by transit.

The downtown design guidelines support and describe design actions that are consistent with the vision, goals and policies of the Silverdale sub-area plan, which was built around Vision 2052, Silverdale Urban Design Study (SUDS), the Central Kitsap School District (CKSD) Youth Summit, and Silverdale Vision 2022.

Key aspects of the Vision 2052 “blueprint vision” are as follows:

Identify the unique amenities that we want to preserve and enhance, ensuring that our community remains a great place to live, work and play.¹

- 1. Urban Form:** To encourage a compact growth pattern that makes wise use of available land and promotes sound investment throughout the community by encouraging infill, redevelopment, and building that goes "up, rather than out," while at the same time preserving marine and mountain vistas.
- 2. Transportation:** To improve the convenience of our road system, reduce congestion, promote alternatives to reliance on the automobile, promote transit opportunities, and create environments that are inviting to pedestrian activity in selected areas of town.
- 3. Quality of Life & Public Safety:** To promote quality housing and encourage the development of safe, distinctive, well-designed, pedestrian-friendly neighborhoods and districts which are provided with adequate public facilities.
- 4. Community Gathering Places:** To develop and maintain urban gathering places that:
 - Provide a venue and focus for Silverdale's civic life and identity, which creates the "sense of community" that serves to anchor citizens.

¹ Silverdale 2022 Plan

-
- Offer educational, recreational, entertainment, and cultural enrichment opportunities for residents of all ages.

5. Quality of Life & Public Safety: To promote quality housing and encourage the development of safe, distinctive, well-designed, pedestrian-friendly neighborhoods and districts which are provided with adequate public facilities. **Community Gathering Places:** To develop and maintain urban gathering places that:

Provide a venue and focus for Silverdale's civic life and identity, which creates the "sense of community" that serves to anchor citizens.

Offer educational, recreational, entertainment, and cultural enrichment opportunities for residents of all ages.

6. Economic Development: To enhance the diversity and strength of Silverdale's economic and employment base, retain our youth, capitalize on our strategic location and promote technology based enterprises that provide good jobs while placing fewer demands on public facilities and services.

7. Parks, Trails & Open Spaces: To promote and enhance the beauty and "walkability" of Silverdale by protecting our tree cover, expanding our parks and scenic open spaces, and providing "non-motorized" links between the parks and greenways and surrounding neighborhoods and districts.

7. Community Character & Design: To build and enhance Silverdale's character and sense of community by setting standards for, and requiring, higher quality design and amenities (e.g., design review; development of pocket parks, courtyards, landscaping and public art).

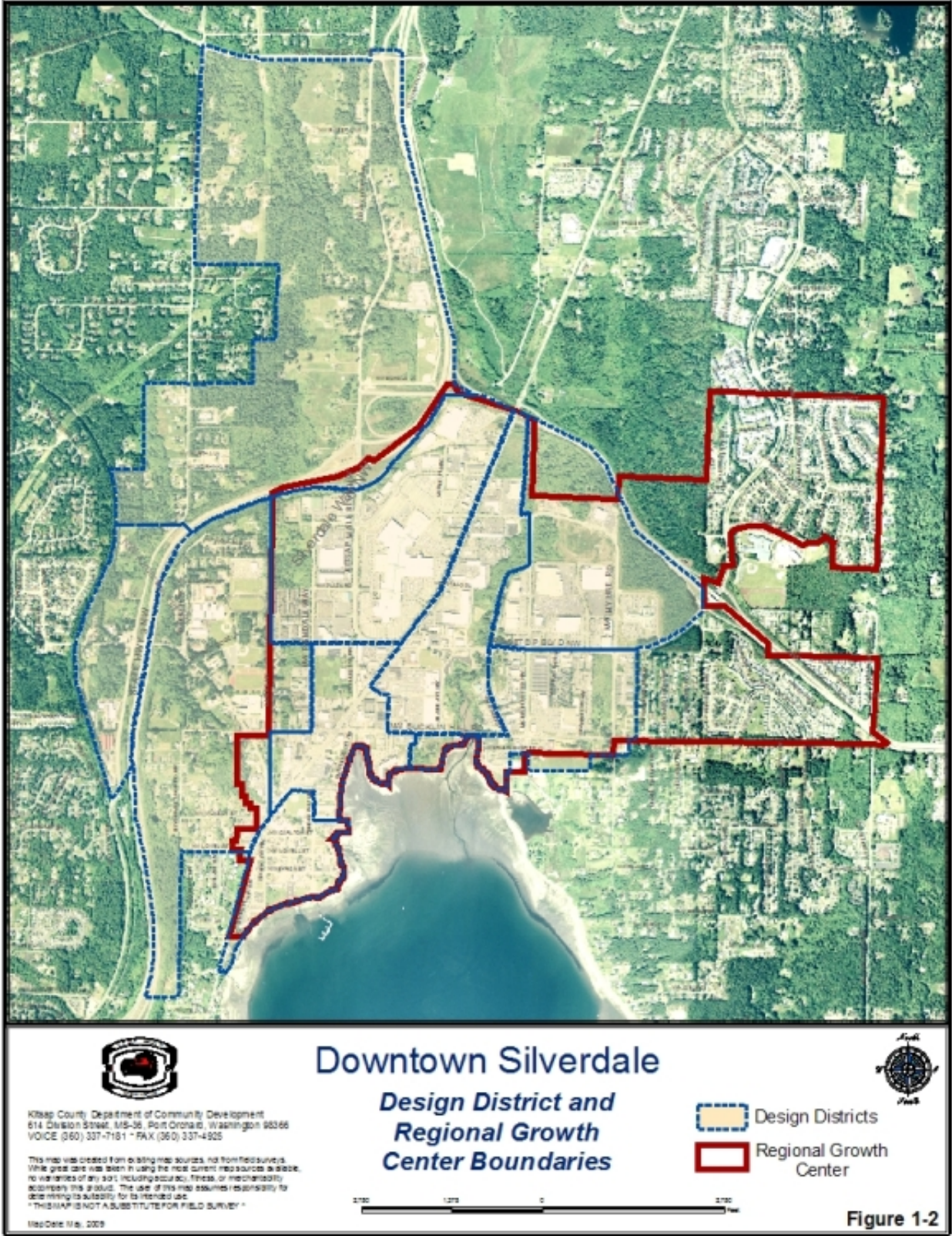
8. Environmentally Sensitive Areas: To preserve and enhance Silverdale's high quality of life by ensuring that new urban development conserves wetlands. Fish and wildlife habitat and maintains surface and ground water quality.

The design guidelines identify "special features" for each design district that describe and articulate the "unique (or special) amenities" contained within each district and strive to implement the above seven vision statements.

1.2 Design Guidelines Approach

The design guidelines present and describe overall urban design intent for Silverdale as a compact, diverse, mixed-use urban center. The guidelines use "unique (special) amenities" of Silverdale as a foundation for design intent and actions, organized in nine districts to focus on physical amenities and features specific to each district.

The Guidelines provide as much detail as possible by district to guide development that is compatible with existing development patterns, incorporates unique or special features, or influences a significant change in those patterns. The methodology is more complex by



intent. It requires extra effort from all stakeholders to assure the implementation of a compact pedestrian-friendly and safe downtown.

1.3 Standards & Guidelines

The Silverdale Design Guidelines are composed primarily of Community Design *Guidelines*—the larger scale design principles that address overall development patterns, circulation, building configurations as well as mass, bulk, height, landscaping and setbacks and the *Standards* that implement the Guidelines. They address how districts relate to one another and the entire downtown, pedestrian and vehicular connection and circulation, and overall design composition within each district.

Mandatory vs. Interpretive Standards/Guidelines

Mandatory refers to the obligatory application of design guidelines and are commonly referred to as standards. The words “will,” “must,” and “shall” indicate a mandatory standard.

Interpretive refers to a translation of a design intent, where a variety of conceptualizations is possible and may require more flexibility in implementation. Words such as “could,” “can,” and “should” indicate a preference, but not a requirement.

1.4 Kitsap County Code & Silverdale Design Guidelines

1.4.1 Applicability. The design guidelines are specific to physical elements and contexts within downtown Silverdale and its districts and apply to all new construction or substantial reconstruction within the Silverdale Design Districts.

- A. For all existing structures in the design districts with exterior improvements or additions or both valued at less than 50 percent of the replacement value of the structure, only the new portion(s) of the structure and associated site improvements are subject to the design guidelines
- B. For all existing structures in the design districts with exterior remodels greater than 50 percent of the replacement value of the structure in any three-year period, then the standards apply to the entire structure and associated site improvements.

1.4.2 Conflicts. Where there is a conflict between the design guidelines and other Kitsap County regulations not associated with the adopted Building Code, the provisions in the design guidelines govern where applicable. If the design guidelines are silent on an issue that is addressed in Kitsap County Code, the code shall be followed.

1.4.3 Interpretation. If there is a question regarding the interpretation of the guidelines, the Director of the Community Development Department is authorized to make an official interpretation.

1.4.4 Appeals. Appeal procedures follow those outlined in KCC Chapter 21.04.

1.4.5 Variations. Variations from the design guidelines may be allowed as specified in KCC Title 17.

1.4.6 Building Heights. Height requirements will be based on the requirements of the underlying zone as outlined in KCC Title 17, except base maximum building height in the MU zone will be 65 feet except where otherwise stipulated in Kitsap County Code. In each design district, the base maximum height will be shown, as well as the maximum height that may be allowed if the increase in height:

- A. is consistent with recommendations of the fire marshal/fire district;
- B. is compatible with surrounding uses and zones or creates a buffer between incompatible uses; and
- C. results in a decrease in building coverage, an increase in public amenities, or a more creative use of land.

1.5 How to Use This Document

The Silverdale design guidelines differ from conventional urban design guidelines by focusing on geographically distinct districts that comprise the Silverdale commercial center, and by using the unique or special features within that district as a basis for many design actions.

To use the report, first refer to the Big Picture in Chapter Two for an overview of building composition, open space and circulation objectives for downtown as a whole. Second, in conjunction with the Site Plan Review Process, refer to the specific design district of interest. If a proposed project straddles the boundaries of two or more districts, the project shall abide by the standards/guidelines that will result in the most cohesive design overall, the Director will assist in making this determination. Third, for each district within the Site Plan/Land Use Review Process, refer to the design guidelines and their subcategories as follows:

1.5.1 Physical Identity Elements & Opportunities

Physical Identity Elements and Opportunities is a summary of the unique or special physical characteristics of the district. These can include creeks, building context, wooded slopes and emerging development patterns. These features set the stage for the application of specific design guidelines.

1.5.2 Design Intent

The design intent is a general statement of the vision or aspired design outcome for a district. There are many acceptable ways for public and private interests to accommodate the design intent, providing stakeholders with options and flexibility for innovation and creativity in achieving the objectives of the design intent.

1.5.3 Design Principles

Design principles are the essential rules or directions for accomplishing the design intent. They address the relationships between and among the physical components of development (streets, buildings, walkways, parking, uses, etc.) rather than detailed design features.

1.5.4 Design Actions

Design actions are the specific ways to accomplish design intent and principles such as:

-
- Compositional structure (how the major physical parts of a district are assembled: allowable building area, street and sidewalk network, and key open space features)
 - Building Orientation, Height and Type
 - Open Space
 - Vehicular Circulation
 - Pedestrian Space and Circulation

1.5.5 Design Examples

In many districts, examples are provided to demonstrate various ways to accomplish the design guidelines. These examples are not mandatory; they are instructional sketches and diagrams, visually portraying ways to accomplish design intent for specific sites.

1.6 Silverdale Commercial Center: An Emerging Downtown

Downtown Silverdale is an unincorporated regional growth center with different types of commercial, office, civic and residential patterns. Taken as a whole, there is not currently a coherent or integrated downtown/community center area because of the historical, incremental growth pattern. However, most of the activities and functions of a downtown and civic/community center are present. Lacking are well-defined centers, a compact human-scale, and strong physical and circulation connectors between and among the various centers and districts. The design guidelines address these issues of coherence and physical integration.

1.6.1 Elements of Community Form

The community form of downtown Silverdale provides a foundation for an underlying design strategy for a coherent and integrated downtown. Silverdale is divided into nine design districts, each with a different set of physical or functional conditions. These form the basis or framework of the downtown Silverdale design guidelines and are summarized below.

1.6.2 Silverdale Geophysical Boundaries

- Downtown Silverdale is located on a small delta or water discharge area from surrounding ridges and hills, forming a triangular shape lowland along the north edge of Dyes Inlet, characterized by two, possibly three, creek drainages (Strawberry Creek, Clear Creek, and a culvert drainage referred to in the handbook as “Silverdale Creek”)
- Limited access Highway 303 to the north and northeast
- Bucklin Hill ridge to the east with intermediate topographical benches in the vicinity of the Northeast Business Park district
- Dyes Inlet in Puget Sound to the south
- West Hill topographical bench between the west side of Silverdale Way and Highway 3; and the source of Strawberry Creek flowing to the southeast through Old Town and into Dyes Inlet

1.6.3 Nine Distinct Districts of Activities & Development Patterns

- Kitsap Mall Center (Kitsap Mall and surrounding shopping outlets including big box retail and shopping plazas).

-
- Old Town (Waterfront area and along Washington Ave NW and Strawberry Creek and portions of Silverdale Creek, emerging from a culvert south of Bucklin Hill Road).
 - Waterfront (Waterfront area and along NW Bucklin Hill Road east of Silverdale Way).
 - Northeast Business District to the north and east.
 - West Hill Neighborhood (residential around Silverdale Loop NW), located on a western rise or topographical bench west of Silverdale Way
 - Clear Creek Village, a corridor formed by the eastern edge of the Kitsap Mall Center and Bucklin Hill Districts, and forming the western edge of the Waterfront and the Northeast Business Districts.
 - Bucklin Hill Center, for the area around the intersection of Silverdale Way and Bucklin Hill Road.
 - Waaga Way Town Center, for commercial areas north of Highway 303 and Anderson Hill Road.
 - Provost Commercial District, for commercial areas west of Highway 3 and south of Anderson Hill Road.

1.6.4 Road Types & Development Patterns

Community Arterials & Boulevards

- Silverdale Way (with connections to Highway 3 with a half diamond interchange on north edge of project area)
- Ridgetop Boulevard NW (with connections to Highway 3 via Kitsap Mall Boulevard and Highway 303 along the north and east boundaries of downtown Silverdale)
- NW Bucklin Hill Road
- Clear Creek Road
- Kitsap Mall Boulevard
- Anderson Hill Road
- Mickelberry Road
- Myhre Road
- Randall Way
- Waaga Way Extension and connector roads

Community Streets	}	
Residential Streets	}	Existing throughout Silverdale
Residential Lanes/Alleys	}	

1.6.5 Physical Connections Between & Among Districts

Connectivity Challenges:

- Major arterials throughout Silverdale act as barriers and edges between districts rather than as connecting features, characterized by five lanes, with limited pedestrian paths that are safe and comfortable.

-
- Shopping plaza, center and mall development types are “islands,” with buildings set back from arterials and surrounded by large parking lots with few if any driveway connections to adjacent parcels.
 - Old Town is a small peninsula oriented to Dyes Inlet and bounded and contained essentially by Silverdale Way and Bucklin Hill Road.
 - West Hill Neighborhood is insulated from the remainder of downtown by a sharp rise in topography along its eastern edge.

Connectivity Opportunities:

- Clear Creek and its riparian area form both a boundary and a connecting open space element in a triangular shape north to south, affecting the Kitsap Mall Center, Clear Creek Village, Northeast Business, Waterfront, and portions of Old Town Districts.
- The waterfront edge of Dyes Inlet is a visual connector as well as partial physical connector for the Old Town and Waterfront Districts.
- Significant land now providing surface parking could provide a future development opportunity to infill and connect development throughout the downtown.

1.7 Changing Nature of Shopping Center Industry: An Opportunity

Shopping centers throughout North America are undergoing significant design changes due to market forces, competition, and aging malls. Two development patterns are emerging as a result of these pressures: mixed-use centers and town centers.

1.7.1 Mixed-use Centers

Mixed-use centers are retail-based developments with residential or office uses or both. They are well-served by transit, with a walkable, pedestrian-oriented environment that provides effective access to goods and services. Residential or office uses are configured in both horizontal developments with separate street access or vertical or above grade configurations with secondary uses above ground level retail or office. A major benefit of mixed-use centers is ease of access between housing, employment, and shopping in a compact active area. Many existing shopping centers are converting to mixed-use centers with peripheral or perimeter commercial developments and open-air pedestrian concourses connecting the core of the older centers to adjacent streets.

1.7.2 Town Centers

Town Centers are mixed-use developments with residential or office/retail uses combined with civic or cultural uses. They often serve as a focus of community life for surrounding areas. They provide basic retail services and gathering or leisure places for local residents. They contain shared parking, provide town squares or village greens as central focus areas, and capitalize on natural environmental features as desired amenities. The residential uses are deemed the “glue” for Town Centers, providing an on-site population. Libraries, community centers, exercise facilities and other public or semi-public uses are often included.

Major components of town or village centers include:

-
- A “main street” forms the heart of the center with two or three story buildings, pedestrian-oriented retail at the ground floor level and offices or residential above or along the periphery.
 - Parking is distributed throughout the site in smaller lots with landscaping and clearly separated pedestrian walkways that connect to buildings.
 - A village green provides passive recreation opportunities as well as outdoor retail sales space.
 - Decreased front and side yard setbacks for non-residential uses generate a compact development pattern.
 - A continuous local or interior street network, public or private, connects the “main street,” buildings, open space, parking to adjacent arterials and local street network.

Chapter 2. The Big Picture

The overall design vision outlines the “big picture” and summary of design intent for Silverdale, composed of the following major components:

- *physical identity elements*--these form the basis for area specific design guidelines
- the overall *preferred physical composition* of downtown (how major components are related and arranged)
- a conceptual downtown *local street network* and associated pedestrian walkways
- preferred *building types and configurations*
- public and private *open space patterns*

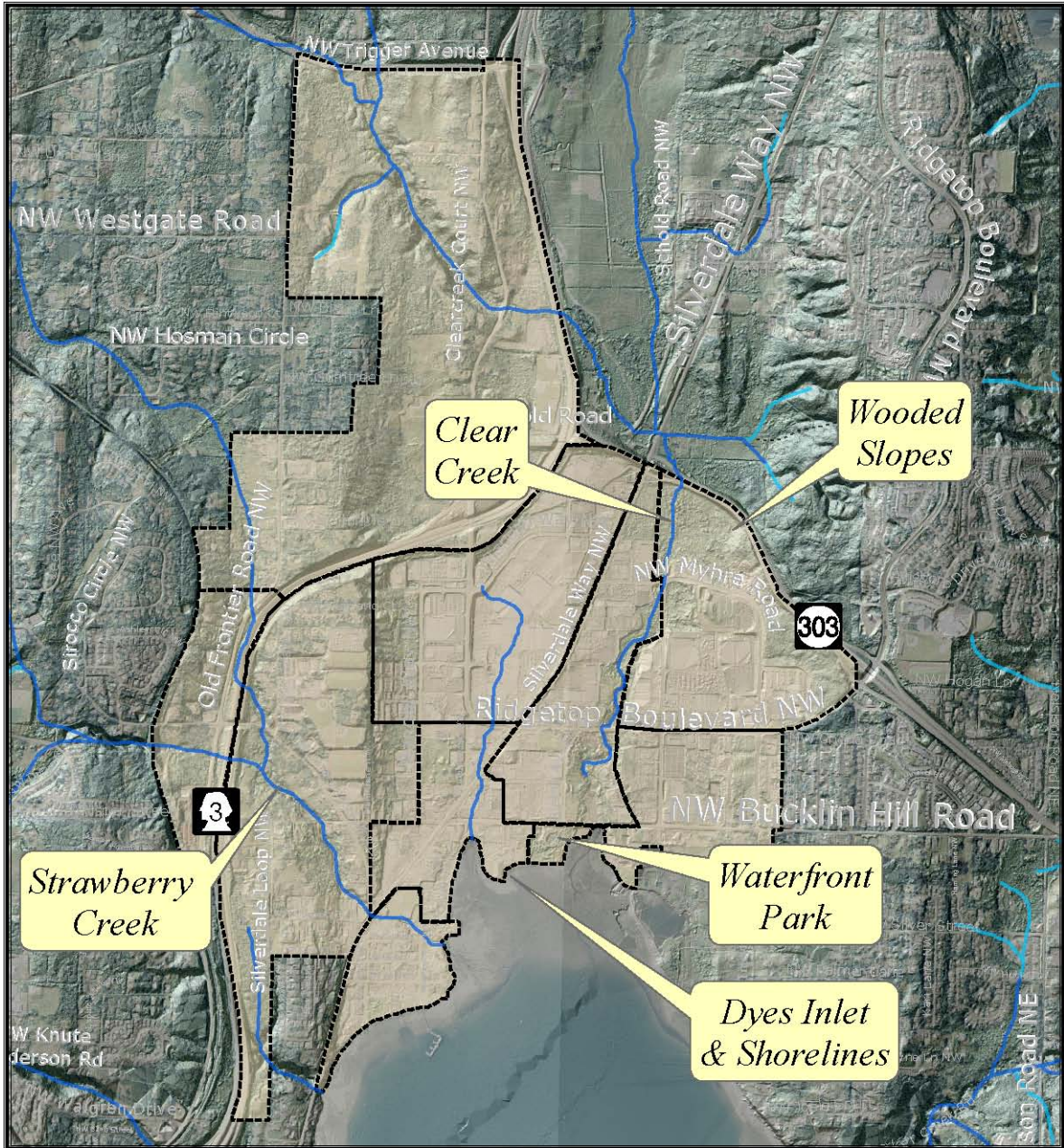
2.1 Physical Identity Elements

Physical Identity Elements identify and describe key natural and built physical elements and activities of each district. These features and elements distinguish districts from others, giving each a special identity. Design guidelines are based on these special features where possible, protecting, enhancing or incorporating these features in a new development pattern.

Figure 2-1 *Silverdale Physical Features* summarizes and describes the key physical characteristics of Silverdale that can positively affect new development patterns in downtown. For example, wooded slopes can be used as buffers between land use or density changes and as natural open space amenities for adjacent developments. Creek corridors can provide visual and environmental amenities for adjacent new developments. Smaller block grid street networks can facilitate pedestrian movement and sidewalk activities. Continuous pedestrian corridors can be accommodated as a part of the larger creek and wooded slope network, connecting major activity centers throughout downtown.

Special physical features of Silverdale, identified as assets and opportunities for the physical identity of Silverdale, include:

- Dyes Inlet and its northern shoreline;
- Clear Creek and riparian corridor extending south into Dyes Inlet from the Silverdale highlands to the west, north and east;
- Wetlands to the north/northeast of downtown, buffering downtown from Highway 303;
- Strawberry Creek and riparian corridor extending south and east into Dyes Inlet from West Hill;
- An underground drainage in a culvert that flows into Dyes Inlet at Bay Shore Drive, referred to in the design guidelines as “Silverdale Creek”;
- Wooded slopes along the eastern edge of West Hill Neighborhood and along the western slopes of lower Bucklin Ridge south of Highway 303;
- Old Town, a compact block grid district along the northwest shoreline of Dyes Inlet; and
- A regional commercial and office center, with Kitsap Mall as the dominant retail type (enclosed mall).



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Map Date: December, 2008

Downtown Silverdale

Physical Features



Downtown Design Districts Boundary

Watercourses (defined in WAC 222-16-030)

Fish Habitat Water Type Code

(S) Designated Shoreline of the State

(F) Fish Habitat

(N) Non-fish Habitat

(U) Unknown, unmodeled hydrographic feature.



Figure 2-1

2.2 Preferred Physical Composition

The physical composition of Silverdale is the manner in which Silverdale is arranged or structured. Key aspects include natural features such as slopes and waterways, well defined historic patterns such as Old Town and its street grid pattern, the overall street network, and the manner by which buildings and open spaces are related, such as mixed use buildings surrounding a village green or pedestrian corridor.

2.2.1 Existing

Excluding Old Town and portions of West Hill, Silverdale is composed of large, super-blocks served by a meandering, partial-grid street network of arterials. The development type or pattern is suburban in nature, i.e., dispersed buildings with adjacent surface parking lots, most with individual access drives from arterials, with buildings set back from the streets and sidewalks. The suburban pattern is represented by Kitsap Regional Mall--an enclosed pedestrian concourse building type surrounded by surface parking lots, big box retail outlets, smaller “L” shaped shopping centers and plazas, and individual free-standing commercial and office buildings with surface parking lots.

Existing development along Clear Creek and its riparian area forms abrupt hard edges along the creek consisting of service drives, parking lots and rear facades of buildings. The creek and riparian area consist of a wedge of open space surrounded by an asphalt plain, separate and unrelated to adjacent development.

2.2.2 Intent

The Silverdale sub-area plan encourages a compact and interconnected pedestrian-oriented downtown and surrounding area, including the following characteristics:

- Protection of the existing compact scale of Old Town;
- Protection and maintenance of natural features such as Clear Creek and its riparian area, Strawberry Creek and an expanded riparian area, enhancement and maintenance of open space and public access within the Dyes Inlet shoreline area, incorporation of wooded slopes into new development in both the West Hill Neighborhood and Northeast Business Park districts;
- Development of a mixed-use village composed of one or more *town center(s)* along Silverdale Way, south of Bucklin Hill Road;
- Bucklin Hill Center, at the intersection with Silverdale Way, composed of an expanded Community Campus, and additional office and lodging activities;
- Clear Creek Village, a mixed use village along both sides of the Clear Creek corridor featuring ancillary residential uses oriented to the creek corridor;
- Kitsap Mall Center, consisting of Kitsap Mall and adjacent retail facilities with improved pedestrian connectors between the mall and adjacent streets, expanded mixed-use activities on and around the mall site, and a slow moving local street system to distribute mall oriented traffic;
- Waterfront District with improved orientation of mixed-use buildings and pedestrian corridors to Dyes Inlet and Clear Creek corridor;
- West Hill Neighborhood, with higher density mixed-use infill development that relates to existing residential patterns and protects and enhances the Strawberry Creek drainage and forested slopes; and

-
- Northeast Business, campus-type development highlighted by buildings set in a wooded landscaped setting, with new buildings oriented toward landscaped quadrangles and other campus-type open spaces.

2.3 Local Street Network

2.3.1 Existing

The major arterial street network consists of Silverdale Way as the primary north to south route, with Kitsap Mall Blvd/Clear Creek Road providing north-south access north of Ridgetop Boulevard; Bucklin Hill Road-Anderson Road and Ridgetop Boulevard provide the east-west routes with interior downtown connections provided by Randall Way, Mickleberry Road, and NW Myhre Road. Extensive land area is occupied by paved parking surfaces, which are used as “connectors” by some.

2.3.2 Intent

The existing super block street network is supported by a slow moving, continuous local street network. The network provides access to and through the interior of the major super blocks, connecting buildings and parking lots to major arterials. The local streets are comprised of public and private streets and lanes with articulated edges (curbs, bollards, and other elements), which separate the traffic lanes from pedestrian sidewalks. The local street network is developed as incremental redevelopment occurs, with each succeeding development connecting to the previous network increment.

Figure 2-2 *Potential Local Street Network* illustrates a suggested conceptual pattern of local streets connected with major arterials; the exact location of the local streets may vary as redevelopment takes place.

2.4 Building Configurations & Types

2.4.1 Existing

Buildings in Silverdale, excluding major portions of Old Town and the West Hill Neighborhood, are shopping malls, shopping centers and plazas composed of one- story commercial buildings oriented to large surface parking areas, and stand-alone commercial or office buildings with parking on two to four sides of the buildings. Pedestrian entrances for most buildings are from the parking lots rather than streets. Most buildings have individual access drives to arterials, are one-story in height with higher buildings at Kitsap Mall, Bucklin Hill Road/Silverdale Way intersection, Northeast Business Park and a scattering throughout downtown.

2.4.2 Intent

Silverdale will have a compact development pattern with building heights ranging from 35 feet to 65 feet depending on district and location or as determined in Kitsap County Code. Residential and office uses can be accommodated in mixed use buildings, with retail and general commercial uses at grade levels. Existing surface parking lots will be reduced in area, with more parking located in and under buildings as well in selected parking structures. Many front and side yard setbacks will be eliminated for the interior of super-blocks where new and reconfigured development will be located along and in conjunction with a local street network. Building types along Clear Creek corridor will

be oriented to the creek, providing private open space courtyards and plazas, which will, at a minimum, visually relate to the creek corridor.

2.5 Open Space Patterns

2.5.1 Existing

Silverdale has an open space pattern represented by natural features such as Dyes Inlet and shoreline, Strawberry Creek and riparian corridor, Clear Creek and riparian corridor, and wooded slopes to the west in West Hill Neighborhood and to the northeast in the Northeast Business Park. The existing Community Campus site provides tree cover and open space. Kitsap Mall provides a privately owned enclosed pedestrian mall and courts for consumers using that facility. There is a deficiency of pedestrian open space in the commercial development pattern of downtown Silverdale.

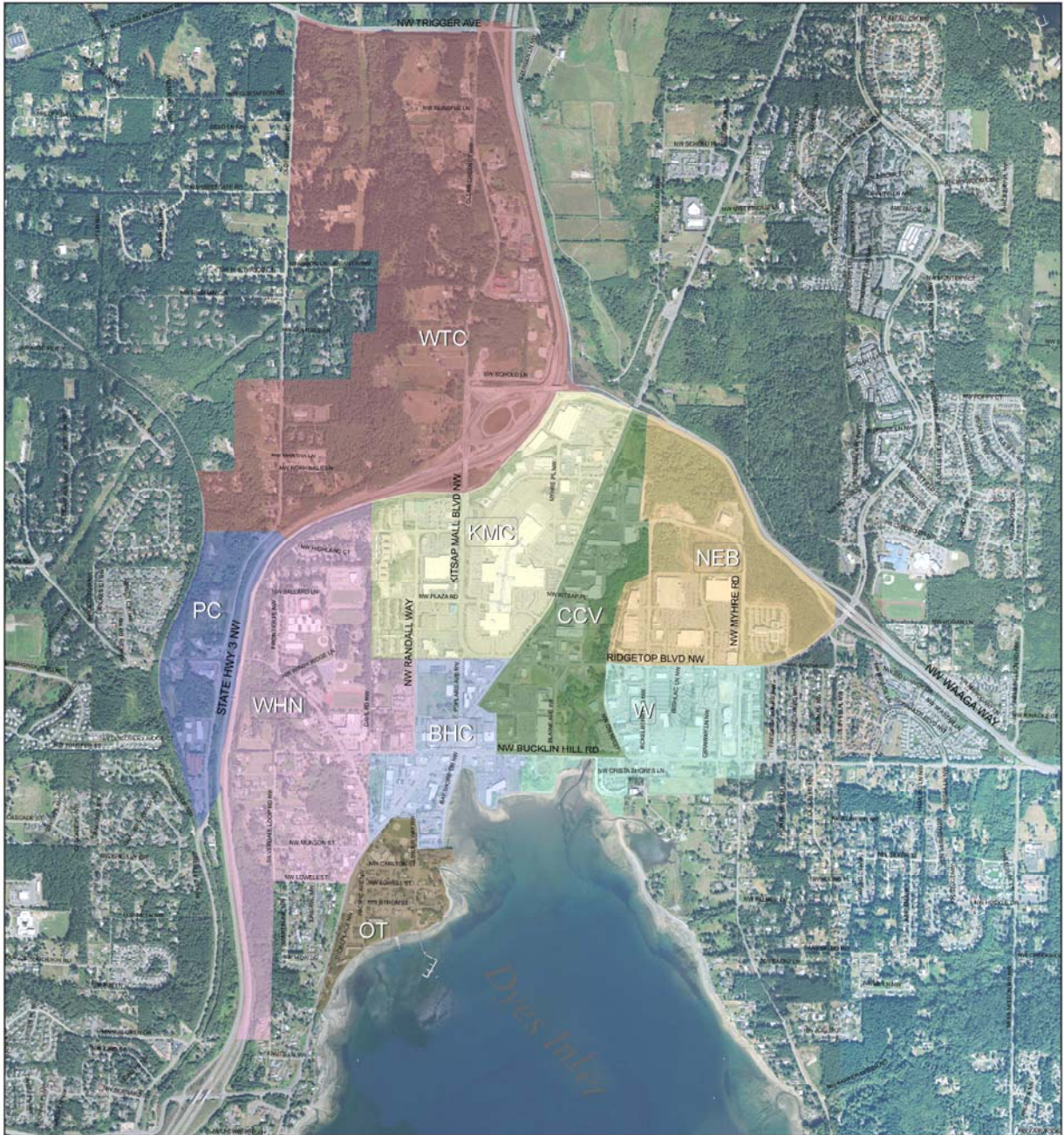
The natural open space pattern forms a north/south “V” shape within Silverdale, providing an existing open space spine that extends from Dyes Inlet on the south edge of downtown to Highway 303. Strawberry Creek drains out of West Hill to and through Old Town and into Dyes Inlet. A drainage channel, named “Silverdale Creek” for reference in the guidelines, enters Dyes Inlet immediately southeast of Bucklin Hill Road and Bay Shore Drive.

2.5.2 Intent

The open space system for Silverdale will protect and expand the natural open space corridors and relate new public and private parks, plazas, green areas and courtyards to the creek corridors, Dyes Inlet and new mixed use residential/commercial developments. The larger open space system, composed of public and private components, will be connected with continuous pedestrian walkways, paths, or trails. Key features of the Silverdale open space systems will include:

- Dyes Inlet and Shoreline, including public access points, parks and shoreline walkways;
- Clear Creek and riparian corridor, including public access or viewing facilities, pedestrian bridge(s) and continuous trails and paths along both sides;
- Strawberry Creek and riparian corridor, including an expanded riparian area in the vicinity of Linder Field and Silverdale Way, public access or viewing facilities, trails and paths along key portions of creek, and public access at the confluence of the creek and Dyes Inlet;
- “Silverdale Creek” protection and expansion through daylighting where feasible as a part of private development north of Bucklin Hill Road;
- Kitsap Mall concourse with potential future expansion to adjacent arterials, either enclosed or semi-enclosed;
- Kitsap Mall community parks incorporated into future mall expansion, located along both the southern and eastern edges of the mall, relating the mall to the adjacent community;
- A diverse distribution of private open space parks, plazas, green areas incorporated into remodeled and new developments and connected to a continuous pedestrian walkway network;

-
- Continuous pedestrian corridors as private development features through major super blocks, connecting, for example, Silverdale Way and Clear Creek corridor and Bucklin Hill Road to Ridgetop Boulevard;
 - Continuous pedestrian walkways and landscaped areas along all major arterials;
 - Landscaped “front yard” pattern in Old Town, West Hill Neighborhood, and Waterfront Districts based on the historic Old Town typology; and
 - “Quadrangle” and courtyard open space features as a part of the Northeast Business District development pattern.



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Map Date: December, 2008

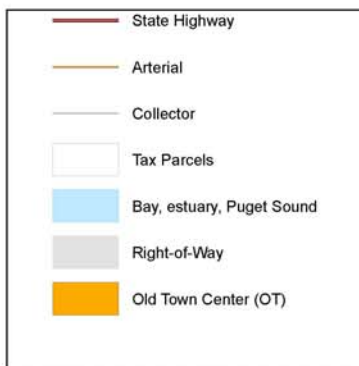
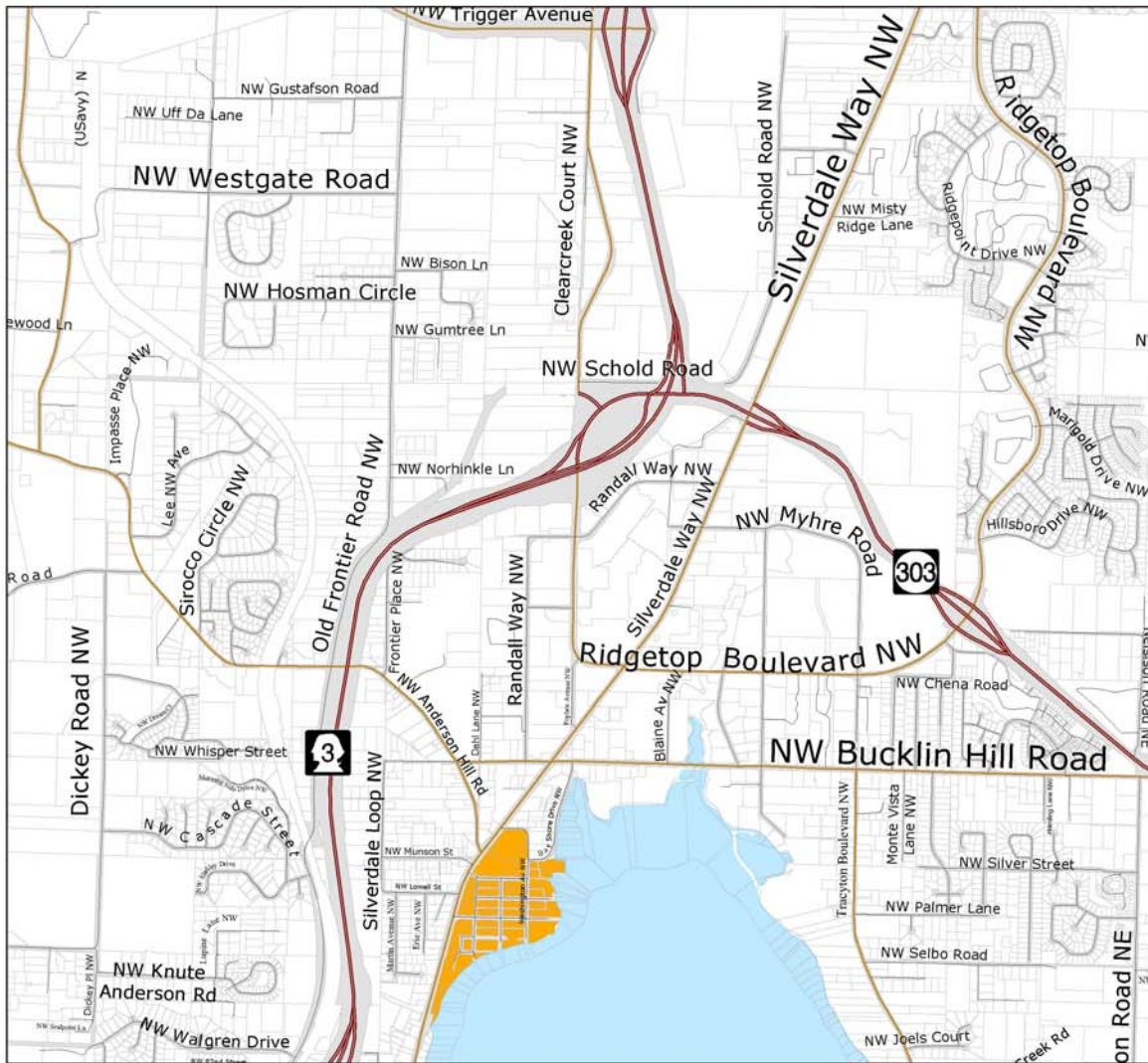
Downtown Silverdale Design Districts

Figure 2-3



- Bucklin Hill Center (BHC)
- Clear Creek Village (CCV)
- Kitsap Mall Center (KMC)
- Northeast Business Park (NEB)
- Old Town Center (OT)
- Provost Commercial District (PC)
- Waaga Way Town Center (WTC)
- Waterfront (W)
- West Hill Neighborhood (WHN)

Old Town Center (OT)



Silverdale Design District



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FIGURE 3-1

Chapter 3. Old Town District

3.1 Physical Identity Elements & Opportunities

Old Town's physical identity is different from the remaining portions of Silverdale. Old Town's physical identities include:

- Block grid street pattern, 200 feet by 300 feet, two way vehicular movement.
- Older one to two story, small-scale commercial and office building types with little or no front yard setbacks.
- Dyes Inlet, shoreline, waterfront parks and views of Inlet.
- Strawberry Creek and riparian area.
- "Silverdale Creek," an underground drainage channel that emerges from a culvert southeast of the Bucklin Hill Road and Bay Shore Drive intersection.
- Linder Field.
- Older, small-scale residential buildings on small blocks with landscaped front yards.
- Older churches and public/semi-public buildings throughout Old Town.

3.2 Design Intent

Modifications to existing buildings and design of new buildings will respect the historic design characteristics of the block grid pattern and adjacent and nearby buildings. The historical relationship of buildings fronting on the street or sidewalk, or the landscaped "front yard" pattern, if applicable, will be maintained in new and reconfigured development. Parking will be located to the rear or side yards or, where appropriate, may be located adjacent to the public roadway, configured as back-in angle parking. The creeks and Dyes Inlet shoreline will be protected and the riparian areas of the creeks will be expanded as major physical features and assets in the development pattern of Old Town.

3.3 Design Principles

- A. The vegetated slopes shall be protected and maintained along both sides of Silverdale Way from approximately NW View Lane north to NW Lowell Street right-of-way.
- B. The Silverdale Way right-of-way shall be landscaped with materials and colors different from other sections of Silverdale Way north.
- C. The Strawberry Creek channel and riparian edge along both sides of Silverdale Way should be expanded.
- D. Key entries to Old Town and its special features should be highlighted with landscape art in the form of sculpture, signs, fabrics and lights.
- E. The small block grid building pattern in the Old Town commercial center, with contiguous buildings along block fronts oriented toward the sidewalk and street rather than parking lots, shall be protected and maintained.
- F. New or redevelopment shall respect existing smaller, non-contiguous development, where appropriate, such as that shown in Figure 3-2 *Smaller-Scale Development*.



Figure 3-2 Smaller-Scale Development

- G. Public access to and along the Dyes Inlet shoreline, including view points where appropriate, shall be maintained and, to the extent possible, expanded.
- H. Pedestrian activities on public and private open space within and along the commercial/office blocks shall be promoted.
- I. Shared off-street parking lots shall be provided in key locations for employee and consumer parking.

3.4 Design Actions

3.4.1 LANDSCAPING, NATURAL FEATURES & SIGNAGE

- A. A flowering tree type or hedge shall be planted along both sides of Silverdale Way from the Downtown Silverdale southern boundary to NW Lowell Street right-of-way on the west and to NW Byron Street on the east.
- B. Where signage/tree conflicts exist with adjacent uses, the trees should be arranged in clusters rather than a linear edge along Silverdale Way.
- C. The Strawberry Creek riparian area has been constricted by adjacent developments and Silverdale Way culverts, increasing the intensity of water flow and safety issues. The Strawberry Creek channel and riparian area on both sides of Silverdale Way could be expanded to reduce water flow intensity and increase the visual amenity of the creek area. Habitat signage, sculpture, and viewing areas along Silverdale Way at the creek could be added. Engineering studies of appropriate safety barriers, which permit visual and/or physical viewpoint access to the expanded creek area, could be conducted.
- D. If the school administration site is redeveloped, the Silverdale Way frontage shall be designed as a landscaped gateway with additional landscape setbacks along Silverdale Way with clusters or rows of ornamental trees.
- E. Old Town logo signage or sculpture could be located at the intersection of NW Byron Street and Silverdale Way, Strawberry Creek, and NW Carlton Street. See Figure 3-3 *Potential Signage* as an example.
- F. If the NW Carlton Street right-of-way is vacated at Silverdale Way, a portion of right-of-way at Silverdale Way shall be used for gateway landscaping and Old Town signage.



Figure 3-3 Potential Signage

- G. As part of a larger gateway design, the feed mill building could be designated as a community architectural asset, encouraging its retention regardless of use changes.

3.4.2 BUILDING ORIENTATION, HEIGHT & TYPES

- A. All buildings shall be oriented to the street grid and sidewalk network with main entry(s) facing the street.
- B. Except for those areas of historical, smaller, non-contiguous development, a continuous and contiguous building façade shall be maintained along the block front adjacent to street and sidewalk, avoiding gaps or “missing teeth” in the front block façade for parking. See Figure 3-4 *Old Town Block Façade Example*.
- C. All commercial buildings shall have ground level transparent front facades oriented toward the pedestrian sidewalks. Figure 3-5 *Example Mixed Use Development* shows this concept.

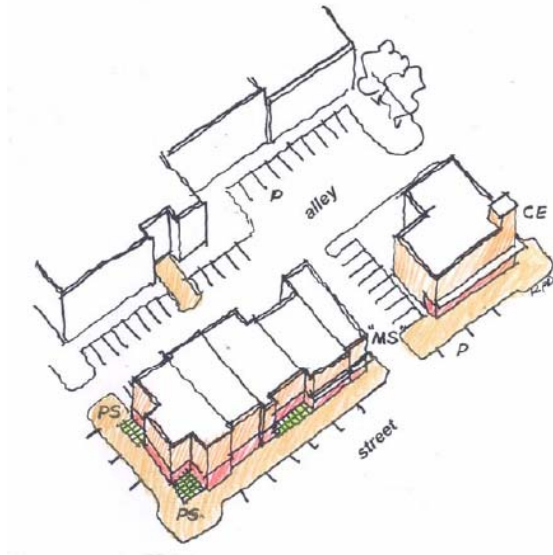


Figure 3-4 Old Town Block Façade Example

- D. Mixed-use development will not be required to have ground floor commercial uses on the building side facing Dyes Inlet.

-
- E. No front or side yard setbacks will be required for new and remodeled buildings.
 - F. Reserve front yards, if any, for landscaping or outdoor consumer-related activities such as sidewalk cafes, coffee shops, restaurants, wine bars, etc., and when provided, do not exceed 12 feet in depth.
 - G. Provide pedestrian weather protection elements (awnings, marquees) where feasible along the front street/sidewalk area, at least 8 feet above the sidewalk.
 - H. New, remodeled or reconfigured buildings in the NC zone will have a maximum base height of 35 feet, with an approved height increase not to exceed 45 feet.
 - I. Live/work buildings may accommodate some or all of the required residential outdoor open space on balconies or decks.

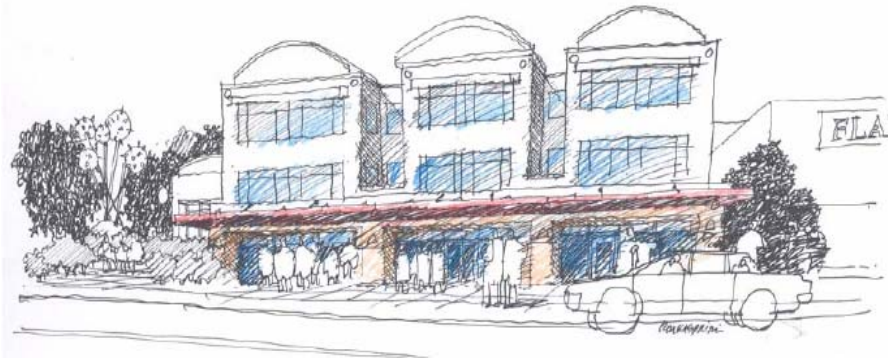


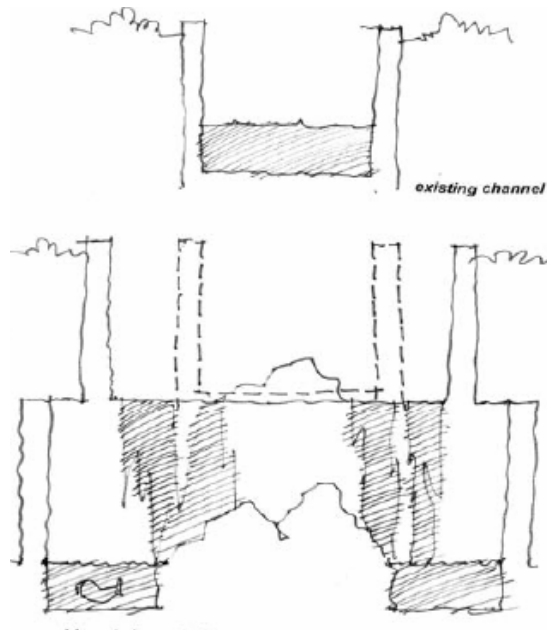
Figure 3-5 Example Mixed Use Development

3.4.3 STREETS & PARKING

- A. No on-site parking requirements, excluding residential uses, for commercial and office uses will be required when a parking development fee, determined by a Business Improvement District (BIA) or, as an alternative, a “fee-in-lieu” program, is paid to a shared-parking fund.
- B. Land can be leased or purchased for shared parking lots using BIA funding mechanisms, as permitted by Washington State.
- C. Off street shared parking lot(s) with a use radius of 800 feet may provide consumer and employee parking for non-residential uses as a means to reduce or eliminate on-site parking.
- D. Parking lots will be landscaped with a four-foot high minimum vegetated edge or buffer between the lot and adjacent sidewalk(s) or with a tree landscaped setback or both.
- E. On-site parking lots shall be located to the rear or side yard or both, accessed by an alley driveway where feasible or by a side yard with a maximum 18 foot wide access drive.
- F. On-street parking may be used to fulfill parking requirements for a proposed use if located on the subject parcel, contiguous with the public right-of-way and meeting all sidewalk and landscape requirements.
- G. Driveway aprons on private property will not be required for alley access parking, enabling private parking space or garage to locate closer to the alley right-of-way line.
- H. On-street parallel parking is allowed and encouraged on all streets.
- I. Provide sidewalks on both sides of all streets, where possible.

3.4.4 STRAWBERRY CREEK PROTECTION & ENHANCEMENT

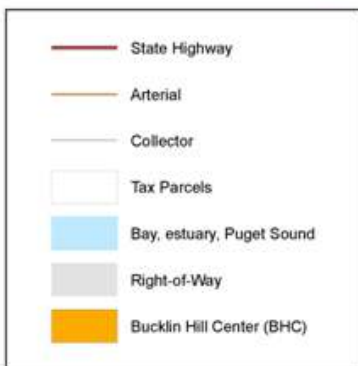
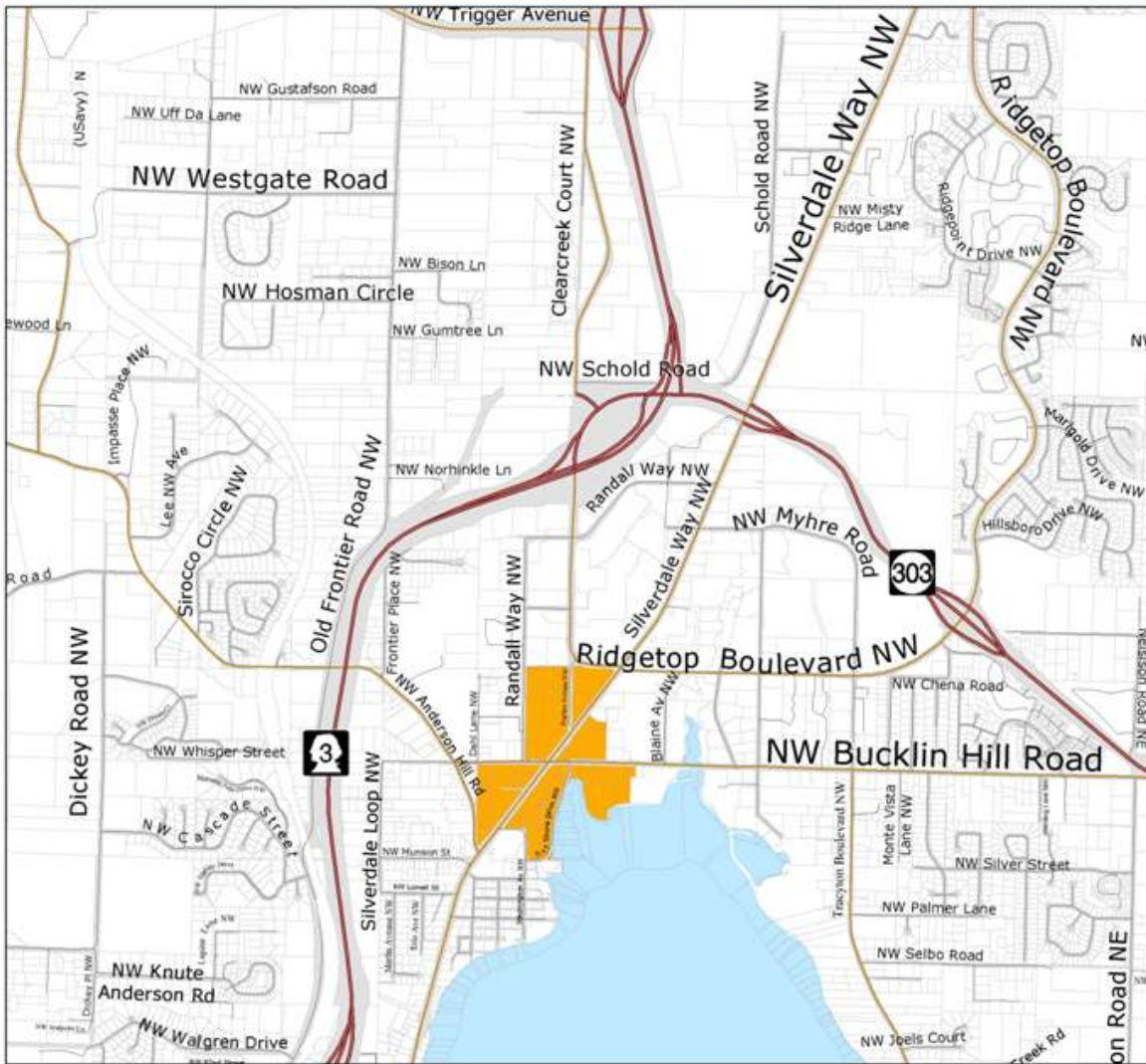
- A. The Strawberry Creek channel is contained by concrete walls and security fencing with little or no riparian area associated with the creek until its confluence with Dyes Inlet. The creek corridor should be opened and expanded to reduce the intensity of water flow and to expand the open space feature of the creek as a major physical identity or signature element of Old Town and Silverdale. Two examples for slowing the water flow and creating a physical amenity include:
 - a. Expanding the creek riparian area horizontally into Linder Field and along the southern edge of the creek, including east of Washington Street.
 - b. If horizontal expansion is restricted, a series of pools as a part of a larger cascade can be developed with a controlled spill way to hold water and release it more slowly into the lower creek, while providing fish access up the cascade.
See Figure 3-6 *Strawberry Creek Slough* as an example.
- B. A transparent security and safety barrier can be provided along the creek, specifications to be determined by appropriate engineering studies.
- C. Strawberry Creek and the Linder Field recreational facility comprise an open space corridor and amenity for the Old Town community, which are improved with a creek-side trail, informational signage and viewing areas.
- D. Strawberry Creek is a fish habitat to be protected and enhanced with side pools, riparian area shading vegetation, a gravel creek bed and other elements determined by fisheries biologists.
- E. The confluence of Strawberry Creek and Dyes Inlet shall be a public access point and attraction with public access and viewing area.
- F. The expanded creek riparian area shall be incorporated into new development on both the school administration site and library site with trails, viewing platforms and seating areas along the creek, with pedestrian connections from adjacent street to the creek area.



Strawberry Creek Slough Diagram illustrates a horizontal expansion of the creek channel (with optional cascade storage pools) to spread and slow the flow of water, establish new habitat, and increase visual open space for the community. Improved security and safety fencing design improves the appearance of the creek corridor while maintaining safety.

Figure 3-6 Strawberry Creek Slough

Bucklin Hill Center (BHC)



Silverdale Design District



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Figure 4-1

Chapter 4. Bucklin Hill Center District

4.1 Physical Identity Elements & Opportunities

Figure 4-1 identifies the boundaries of this district. Historical development occurred in Old Town and south of Bucklin Hill Road along Silverdale Way prior to the expanded shopping center development to the north. Bucklin Hill Center, the area around the intersection of Bucklin Hill Road and Silverdale Way, is both a design district at a major intersection and a gateway area into the larger downtown Silverdale area.

An existing Silverdale Village shopping center is located west of Silverdale Way and another smaller center is located on the east side. A higher intensity of office and lodging uses exists in the northeast and southeast quadrants of the Silverdale Way and Bucklin Hill Road intersection.

The district also includes the Silverdale Community Campus area, bounded by Randall Way on the west, Kitsap Mall Boulevard to the north, Silverdale Way on the east and Bucklin Hill Road on the south. This “community campus” area is expected to provide public amenities such as senior housing, community center and open space, and will be treated separately in this chapter.

4.2 Design Intent

The existing shopping centers located along Silverdale Way will redevelop as mixed-use “village centers” from Anderson Hill Road and Linder Way on the south to Bucklin Hill Road on the north. They will contribute to a larger village character by locating pedestrian squares in visual proximity to one another along and visible from Silverdale Way, by connecting local service streets to other adjacent districts where feasible, and by providing safe pedestrian connectors across Silverdale Way.

New interior local streets will provide a continuous traffic pattern into and through the village. The mixed-use developments will add residential or office space to retail uses, incorporate civic or cultural uses where feasible, and integrate an expanded pedestrian open space system to parking areas, retail facilities and other uses.

Above grade housing or office uses reinforce the retail uses at ground level. The western edge of the existing Silverdale Village borders a wooded slope, providing an opportunity for medium density housing in that area. Housing provides a transition use between the adjacent West Hill Neighborhood and the village. It provides opportunities for pedestrian access from West Hill to the Silverdale Village Center with a West Hill climb incorporated into new development, and provides views of Dyes Inlet. Similarly, the eastern edge of the eastern village component borders a portion of Old Town and has potential views of Dyes Inlet, Linder Field and Strawberry Creek.

A mixed-use area of larger scale office, lodging, and entertainment buildings could be located in the northeast and southeast quadrants of the Silverdale Way and Bucklin Hill Road intersection and along the shore lands on the south side of Bucklin Hill Road.

4.3 Design Principles

Former shopping plazas should be converted to mixed-use centers, comprising a village center complex on both sides of Silverdale Way, referred to as *Silverdale Village*. See Figures 4-2 and 4-3 for design sketches of a sample “village.”

The basic elements and composition of the town or village center include:

- A. A “main street” will be part of a private or public local service street network for each side of Silverdale Way, providing clearly defined on-site vehicular circulation, connecting on-site parking areas to major arterials and adjacent districts where feasible.
- B. Local service street access to perimeter arterials will be coordinated with adjacent and nearby developments as development incrementally occurs.
- C. One to four story buildings shall be arranged along a “main street” and clustered around a village square and smaller pedestrian courtyards.
- D. Pedestrian walkways, sidewalks, and other paths will provide a continuous pedestrian connection from on-site facilities to parking, adjacent public streets and nearby developments.
- E. Housing or office units will be mixed with retail uses, either stacked above or clustered next to retail and located away from Silverdale Way where feasible.
- F. A village square, plaza, greens, or commons shall be provided in a centralized location, along “main street,” as a focal leisure space for each development, integrated with ground floor retail and residential/office uses, and oriented to the southeast, south, or southwest.
- G. Parking areas will be in small, dispersed parking lots.
- H. One connecting east-west vehicular crossing with a signalized intersection can occur in the southern portions of each development.



Figure 4-2 Village Example



Figure 4-3 Village Example

Basic elements of design in the existing higher density area east of Silverdale Way would include:

- I. Parking and vehicular access will be from a local street network with controlled access points to arterials safely away from the Bucklin Hill/Silverdale Way intersection.
- J. Development on the south side of Bucklin Hill Road shall orient to Dyes Inlet and Silverdale Creek with landscaping and public pedestrian access facilities along the shoreline.

4.4 Design Actions

4.4.1 BUILDING ORIENTATION, HEIGHTS & TYPES

- A. Mixed-use buildings will be oriented to a “main street” and village square, with front and side yard setbacks ranging from zero to 8 feet maximum depth along major pedestrian walkways.
- B. Civic or cultural buildings, if any, should have prominent locations, preferably at major intersections or on village square.
- C. New, remodeled or reconfigured buildings in the RC zone will have a maximum base height of 35 feet, with an approved height increase not to exceed 45 feet. Residential units should be located away from Silverdale Way and Bucklin Hill Road.
- D. Weather protection features such as canopies and marquees shall be provided on storefront buildings along major pedestrian walkways.
- E. Commercial buildings at prominent corners shall have distinctive architectural features such as setback entries, corner towers, or architectural sculpture
- F. Live/work buildings can provide a transitional use and building type between commercial and residential buildings.

4.4.2 OPEN SPACE & PEDESTRIAN SYSTEM

- A. Pedestrian-oriented village squares, greens, commons, courtyards, and walkways will be incorporated into mixed-use developments. They will contain a variety of hard and vegetated surfaces and will accommodate outdoor activities ranging from cafes and coffee shops to outdoor markets, sales and play areas. See Figure 4-5 for an example of a village square with mixed-use buildings.



Figure 4-4 Village Square

- B. The village square or green should be the largest and central space located on “main street,” with size varying according to site conditions and size of development, e.g., 7,000 to 8,000 square feet (approximating the size of a volleyball court) can provide space for diverse outdoor activities.
- C. Commercial Courtyards, between 30 to 50 feet in width, shall be provided in or between retail clusters, with buildings on two or three sides.
- D. Live/work buildings may accommodate some or all of the required residential outdoor open space on balconies or decks.
- E. Pedestrian promenades and “main street” walkways that connect major components of the village development and the village to perimeter arterials, will be 12 feet minimum in width for the walking area on at least one side of the street, and 8 to 12 feet on the remaining side. Where outdoor activities such as cafes, restaurants, etc. are anticipated to use portions of the walkways, add 8 feet to the sidewalk width. Where parking abuts the “main street” walkway, add 4 feet minimum for planting features or 2 feet minimum if vertical barriers such as artistic bollards, attractive hardscape elements or artistic features.
- F. Parking lot walkways that connect parking lots and lots to retail clusters will be 8 feet minimum width.
- G. Pedestrian crossings within a parking lot will be 6 feet minimum in width and either striped, containing different paving materials for contrast or raised.
- H. Weather protection features such as canopies and freestanding shelters should be dispersed throughout parking areas for pedestrian comfort and safety.
- I. Two pedestrian highway crossings should be located along Silverdale Way south of Bucklin Hill Road and should connect mixed-use centers east and west of the highway, using pedestrian-activated signals, raised medians, pedestrian flags, different paving patterns or flashing lights.

-
- J. A pedestrian bridge should cross Silverdale Creek, south of Bucklin Hill Road, connecting Bay Shore Drive with the waterfront walkway on the east side of the creek.
 - K. A waterfront walkway will extend along the waterfront east of Silverdale Creek, from Bucklin Hill Road south and eastward to the county park and Clear Creek corridor.
 - L. If feasible, a portion of "Silverdale Creek," a covered drainage channel flowing into Dyes Inlet, will be daylighted on the north side of Bucklin Hill Road and incorporated into new or expanded development. Reduced parking requirements, variations in building heights, or reductions in permit fees may be granted to a development that incorporates an open creek in its site design.

4.4.3 STREETS & PARKING (See Figure 4-5 *Parking, Landscape and Street Diagram* for examples)

- A. "Main streets" and collector street(s) connecting the development to major arterials will be 22 to 24 feet in width with parallel parking places on one or both sides of street, with a raised or protected sidewalk (bollards, planting features) on both sides of street.
- B. Secondary streets connecting parking lots to main streets and collector streets will be 20 to 22 feet in width with a raised or protected sidewalk (bollards, planting features) and a parking lane on at least one side of street.
- C. Non-signalized streets connecting to arterials will have limited turning movements such as right turn in and right turn out, keeping with the flow of arterial traffic.
- D. Local streets that serve as delivery lanes may use "main street" dimensions.
- E. Parking areas will be small clusters of lots consisting of one to two double-loaded parking bays maximum, i.e., one driving lane flanked by one row of parking stalls on each side.
- F. On-street parking may be used to fulfill parking requirements for a proposed use if located on the subject parcel, contiguous with the public right-of-way and meeting all sidewalk and landscape requirements.
- G. Individual parking clusters shall be separated by an 8 feet wide minimum sidewalk protected by curbs, plantings or artistic bollards.
- H. Parking lots and structures in the northeast quadrant of the Bucklin Hill Road and Silverdale Way intersection will be accessed from a local street network to the north, northeast and east of the intersection development.

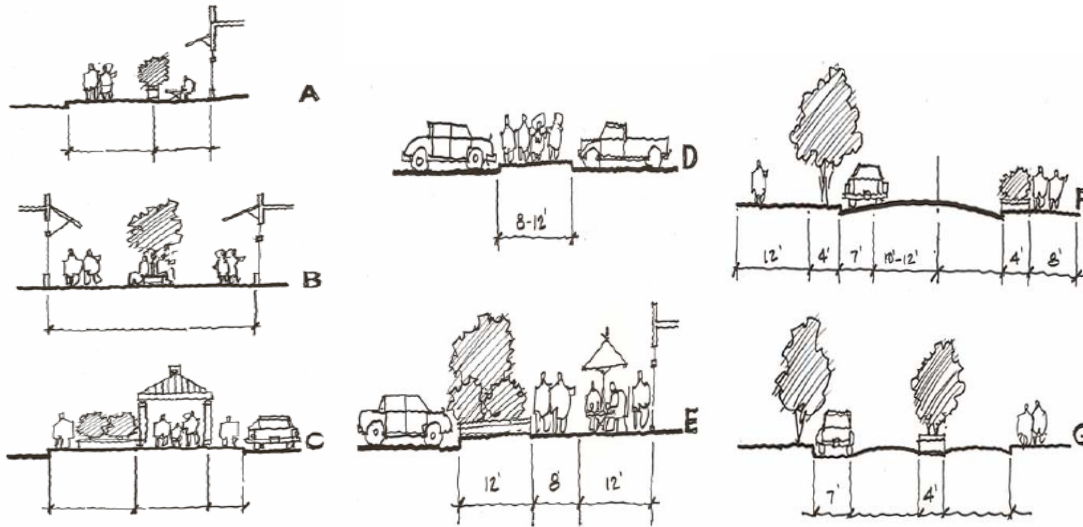


Figure 4-5 Parking, Landscape and Street Diagram

4.A.1 SILVERDALE COMMUNITY CAMPUS

4.A.2 Design Intent

The Silverdale Community Campus will expand to include a campus type pattern. Coordination of Community Campus open space location and design with that of adjacent properties may establish a physical gateway for the larger downtown area, providing the Community Campus with a strengthened presence along Silverdale Way. New and remodeled development within the properties in the northwest quadrant of the Silverdale Way/Bucklin Hill Road intersection, including the campus, and at the Poplars Motel site, will all contribute landscape and setback features to the gateway design. The campus and the Kitsap County Consolidated Housing Authority (KCCHA) site open space designs will be visually connected across Poplars Avenue, creating a landscaped foreground for both developments and downtown.

4.A.3 Design Principles

- A. The Community Campus will be arranged around an interior open space village green, open to the southeast toward Silverdale Way and the KCCHA site, with strong pedestrian connections.
- B. The KCCHA site should incorporate the southern portion of its property into the gateway design.
- C. The Silverdale Way right-of-way will contribute special open space features as a part of the larger gateway design.
- D. Weather protection features such as canopies and marquees shall be provided on buildings along major pedestrian walkways.

4.A.4 Design Actions

1. COMPOSITIONAL STRUCTURE

-
- A. New facilities will be built around a central village green, with the green connected physically and visually to Poplars Avenue and the KCCHA site and Silverdale Way.
 - B. The KCCHA site may contain a landscaped open space on the southern portion of the site as a part of the campus and downtown gateway: signage, art and landscaping could all highlight this open space feature.

2. BUILDING ORIENTATION, HEIGHTS & TYPE

- A. The Community Campus will organize new facilities around a village green with trees, seating areas, outdoor meeting places, sculpture with an orientation of the courtyard to Poplars Avenue and the KCCHA site.
- B. The KCCHA site, when fully redeveloped, may contain a residential building(s), possibly mixed-use with retail or offices on the ground floor, oriented to the landscaped southern portion of the lot and Silverdale Way as a part of a larger gateway design, referred to as Gateway Green.
- C. Buildings will be 65 feet maximum height.

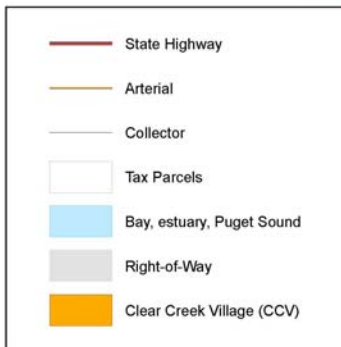
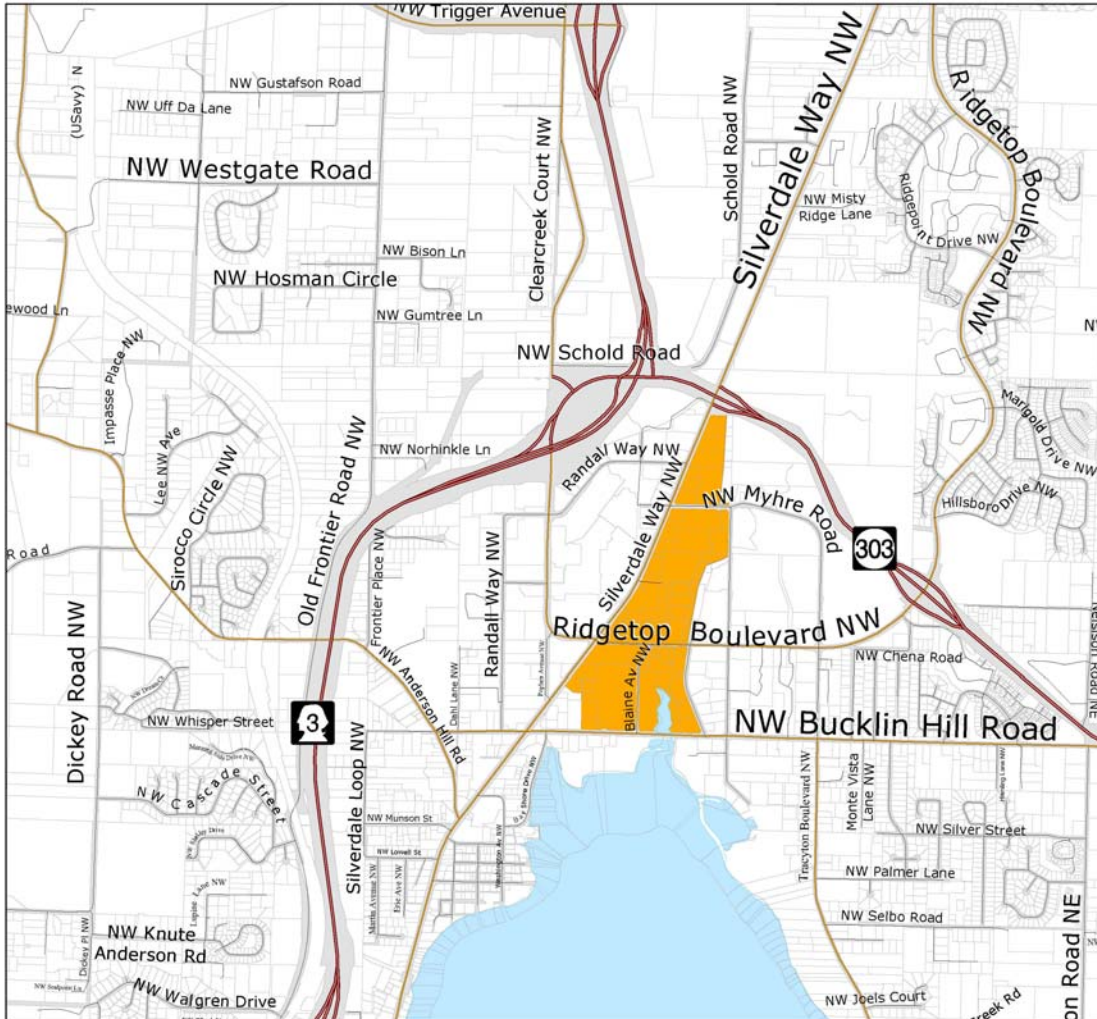
3. OPEN SPACE & PEDESTRIAN FEATURES

- A. A central village green will be the focus of new campus buildings and may contain features such as landscaping, seating areas, outdoor meeting places, and sculpture.
- B. The campus green will be connected to Poplars Avenue and shall have a minimum 10 feet wide hill climb with handrails, landings and seating.
- C. If Poplars Avenue is not vacated as a street, a crosswalk will connect the campus hill climb to the KCCHA site, with the crosswalk consisting of a raised pedestrian crossing or a contrasting paving pattern a minimum of 12 feet in width.
- D. The KCCHA site shall contain a landscaped open space, Gateway Green, a minimum of 30 feet along Poplars Avenue and Silverdale Way from the southern tip of the property as the site is redeveloped. If provided, the area could be traded for lesser setbacks and landscape areas on other portions of site.
- E. The KCCHA Gateway Green may contain sculpture related to the campus and the larger downtown gateway plus appropriate signage.

4. PARKING FEATURES

- A. Surface parking facilities shall be dispersed around the Community Campus site in small lots (one double-loaded bay configuration), with no surface lots between Poplars Avenue and the courtyard facing Poplars Avenue.
- B. A proposed parking structure will be located on the site in the most appropriate location.
- C. Parking for the KCCHA site shall be to the northwest and north portions of the site, north of the Gateway Green.

Clear Creek Village (CCV)



Silverdale Design District



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Figure 5-1

Chapter 5. Clear Creek Village District

5.1 Physical Identity Elements & Opportunities

Clear Creek Village extends north from Bucklin Hill Road to State Highway 303, excluding the northeast portion of the Bucklin Hill Road/Silverdale Way intersection, and from Silverdale Way eastward to and including eastern portions of the Clear Creek corridor. Clear Creek Village is comprised of a riparian area, a north-south greenway forming a natural “V” shape through the center of the Silverdale commercial center. Medium-sized, one-story shopping center/plazas with front yard surface parking lots, capable of being reconstituted as mixed-use centers are the existing development pattern. Silverdale Way is the western boundary of the super blocks north of Bucklin Hill Road to State Highway 303. Significant east-west depth of super blocks from Silverdale Way to Clear Creek provides a potential development amenity for residential units located along the creek corridor and away from arterials.

5.2 Design Intent

Clear Creek Village will be a new and central neighborhood or village composed of a series of interconnected mixed-use developments along the west side and immediate east side of Clear Creek riparian area. The village will include residential, commercial and office uses arranged around village squares, connected by continuous pedestrian walkways and a local street network. New and reconfigured developments will use the Clear Creek corridor as an open space amenity and design feature. Residential uses will orient to the creek corridor, visually connecting private open space features with the creek corridor. Development within the village will be oriented toward a local street network. See Figure 5-1 *Clear Creek Village Site Design Example* and 5-3 *Clear Creek Village Streetscape Example*.

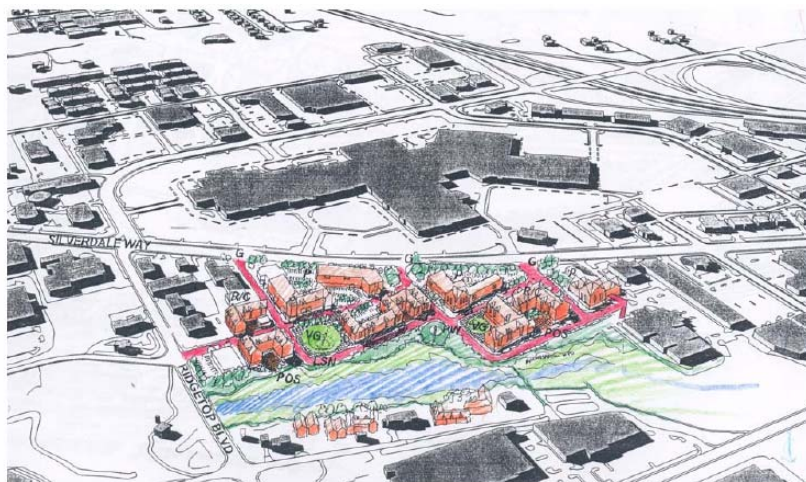


Figure 5-2 Clear Creek Village Site Design Example

5.3 Design Principles

- A. As new and reconfigured commercial developments occur, a compact, incremental building pattern with an interconnected local street network will emerge.
- B. New mixed-use residential/commercial developments will be oriented along the eastern portion of the Clear Creek Village, focusing on Clear Creek and riparian area.
- C. Mixed-use centers will provide a hierarchy of open space features for residents and consumers, including at least one village square or commons, courtyards, wider sidewalks for outdoor commercial activities, and small pedestrian “eddies,” i.e., places for shoppers to pause and rest.
- D. Village squares or gathering places will be interconnected with a village-wide pedestrian walkway system, including a Clear Creek trail system.
- E. Mixed-use centers will have at least one major pedestrian promenade connecting the development to west and south arterials, and interconnecting with adjacent developments to achieve the arterial connections.
- F. Mixed-use developments along Clear Creek will orient private open spaces to the creek corridor and trail system, at least visually, where feasible.
- G. Vehicular access will be limited to full or partially controlled intersections from Silverdale Way and Bucklin Hill Road, connecting to an interior local street network.



Figure 5-3 Clear Creek Village Streetscape Example

5.4 Design Actions

5.4.1 COMPOSITIONAL STRUCTURE

- A. Development along the Clear Creek corridor, with mixed-use residential and commercial buildings and related open space, will face east, southeast and west.
- B. A local service street network, interconnected among various developments, will provide an informal or flexible street grid for access and interior block circulation.
- C. A “main street” will connect the different components of the village to adjacent arterials as development occurs.
- D. The street system will be complemented by a continuous pedestrian walkway system connecting mixed-use centers to the creek corridor and adjacent arterials.

5.4.2 BUILDING ORIENTATION, HEIGHTS & TYPES

- A. Developments along the local street network will have no front and side yard setback requirements.
- B. If a front or side yard setback is included in the site design, the setback will be 8 feet maximum in depth and pedestrian or landscape in function.
- C. Civic and/or cultural buildings, if any, should have prominent locations, preferably at major intersections or on village square.
- D. The retail component should be prominent and visible from the arterial(s) or located on “main street” or both.
- E. New, remodeled or reconfigured buildings in the RC zone will have a maximum base height of 35 feet, with an approved height increase not to exceed 65 feet, except where authorized by Kitsap County Code.
- F. Residential units shall be located in relationship to an amenity such as natural open space, a village square, “main street” or courtyards and away from Silverdale Way and Bucklin Hill Road.
- G. Weather protection features such as canopies and marquees shall be provided on storefront buildings along major pedestrian walkways.
- H. Commercial buildings at prominent corners should have distinctive architectural features such as setback entries, corner towers, or architectural sculpture
- I. Live/work buildings can provide a transitional use and building type between commercial and residential buildings.
- J. Buildings along the Clear Creek corridor will orient courtyards and landscaped setbacks to the creek and related trails and walkways.

5.4.3 OPEN SPACE & PEDESTRIAN SYSTEM

- A. Pedestrian-oriented village squares, greens, commons, courtyards, and walkways will be incorporated into mixed-use developments. The spaces should contain a variety of hard and vegetated surfaces and accommodate outdoor activities ranging from cafes and coffee shops to outdoor markets, sales and play areas.
- B. The village square or green shall be the largest and central space located on “main street,” with size varying according to site conditions and size of development, e.g., 7,000 to 8,000 square feet (approximating the size of a volley ball court) can provide space for diverse outdoor activities.
- C. Commercial Courtyards, between 30 to 50 feet in width, shall be provided in or between retail clusters, with buildings on two or three sides.
- D. Live/work buildings may accommodate some or all of the required residential outdoor open space on balconies or decks.
- E. Private courtyards and open spaces along the creek may be secured with fences, hedges and other security edges, while maintaining a visual connection to the creek corridor.
- F. Pedestrian promenades and “main street” walkways, which connect major components of the village development and the village to perimeter arterials, will be 12 feet minimum in width for the walking area on at least one side of the street, and 8 to 12 feet on the remaining side. Where outdoor activities such as cafes, restaurants, etc. are anticipated to use portions of the walkways, add 8 feet to the

-
- sidewalk width. Where parking abuts the “main street” walkway, add 4 feet minimum for planting features or 2 feet minimum if vertical barriers such as artistic bollards, attractive hardscape elements or trees are provided.
- G. Parking lot walkways that connect parking lots and lots to retail clusters will be 8 feet minimum width.
 - H. Pedestrian crossings within a parking lot will be 6 feet minimum in width and either striped, containing different paving materials for contrast or raised.
 - I. Weather protection features such as canopies and freestanding shelters should be dispersed throughout parking areas for pedestrian comfort and safety.
 - J. Pedestrian highway crossings shall be located across Ridgetop Boulevard and Bucklin Hill Road and should connect mixed-use centers north and south, using pedestrian-activated signals, raised medians, pedestrian flags, different paving patterns or flashing lights.
 - K. The Clear Creek Corridor should have a 6 feet wide minimum pedestrian walkway/trail network along both sides of the creek, with pedestrian bridges where feasible.

5.4.4 STREETS & PARKING

- A. “Main streets” and collector street(s) connecting the development to major arterials will be 22 to 24 feet in width, with parking spaces on one or both sides of street, and a raised or protected sidewalk (artistic bollards, planting features) on both sides of street.
- B. Secondary streets connecting parking lots to main streets and collector streets will be 20 to 22 feet in width with a raised or protected sidewalk (bollards, planting features) and a parking lane on at least one side of the street.
- C. Non-signalized streets connecting to arterials will have limited turning movements such as right turn in and right turn out, keeping with the flow of arterial traffic.
- D. Local streets, which serve as delivery lanes, may use “main street” dimensions.
- E. Parking areas will be small clusters of lots consisting of one to two double-loaded parking bays maximum, i.e., one driving lane flanked by one row of parking stalls on each side.
- F. On-street parking may be used to fulfill parking requirements for a proposed use if located on the subject parcel, contiguous with the public right-of-way and meeting all sidewalk and landscape requirements.
- G. Individual parking clusters shall be separated by an 8 feet wide minimum sidewalk protected by curbs, plantings or artistic bollards.
- H. Parking for creek-side development on the east side of creek shall be away from the creek. Parking in the setback facing the street shall be 50% or less of the parcel width.

Chapter 6. Kitsap Mall Center

6.1 Physical Identity Elements & Opportunities

Kitsap Mall is a regional shopping center with an enclosed pedestrian concourse and significant open space in the form of surface parking lots adjacent to the mall. The center borders the west edge of Silverdale Way for approximately 1,200 feet, presenting an opportunity for future building and landscape design that can relate directly to Silverdale Way and Clear Creek Village.

6.2 Design Intent

Kitsap Mall Center will continue to be a regional shopping center with expanded functions to include office, retail, residential and a transit hub. Kitsap Mall itself will expand as a mixed-use center, connecting its pedestrian concourse with Silverdale Way, Kitsap Mall Boulevard, and Randall Way NW at a minimum. Infill development will relate building activities and orientation to both the expanded interior/exterior pedestrian concourse and to Silverdale Way. The southern and eastern portions of the mall site will contain landscaping and pedestrian features that physically and visually connect the mall to Silverdale Way and the greater downtown Silverdale area to the south and southeast, providing significant private/public community gathering places.

6.3 Design Principles

- A. Kitsap Mall will expand its pedestrian/shopping concourse in at least three directions (north, east, south) as a combined enclosed/open shopping “street” connecting directly to Silverdale Way, Kitsap Mall Boulevard/Silverdale Way intersection area, and Randall Way.
- B. Kitsap Mall will incorporate a community green space or village square at the southeast portion of the site as a part of mixed-use expansion.
- C. Kitsap Mall will incorporate a village square along its Kitsap Mall Boulevard and Silverdale Way edges.

6.4 Design Actions

6.4.1 COMPOSITIONAL STRUCTURE

- A. Kitsap Mall will expand to form a cross shape extending to adjacent arterials and infilled with new mixed-use and parking structures.
- B. The pedestrian concourse will serve as a physical circulation spine that connects the existing concourse to new and outlying enclosed and open pedestrian walkways and community green spaces.
- C. Community green spaces will occur at the southeasterly section along Kitsap Mall Boulevard and Silverdale Way, with direct access and connection to the arterial and sidewalks, and at the easterly entrance to the mall, providing a significant front yard private/public gathering area.
- D. Parking structures could be accommodated along the western and northwestern sides of the mall with direct connection to the pedestrian concourse.
- E. At least one major landscaped formal entry drive shall connect the mall complex to Silverdale Way.

6.4.2 BUILDING ORIENTATION, HEIGHTS & TYPE

- A. Buildings should be oriented to a combined enclosed/open pedestrian concourse and new interior block driving lanes, which expand the pedestrian and vehicular movement patterns within the mall.
- B. Buildings should be oriented around community green space(s), especially along the southern and eastern portions of the mall site.
- C. New, remodeled or reconfigured buildings in the RC zone will have a maximum base height of 35 feet, with an approved height increase not to exceed 65 feet except where authorized by Kitsap County Code. Building types vary from large commercial buildings to stacked multi-story office and residential buildings.

6.4.3 OPEN SPACE & PEDESTRIAN CIRCULATION

- A. At least two community green spaces shall be located along the south and east edges of the mall.
- B. Each green space shall be a leisure or lingering space related directly to adjacent shops and serve as a “town square.”
- C. Smaller pedestrian plazas and resting places shall be located throughout the development, connected by a pedestrian concourse and collector sidewalks.

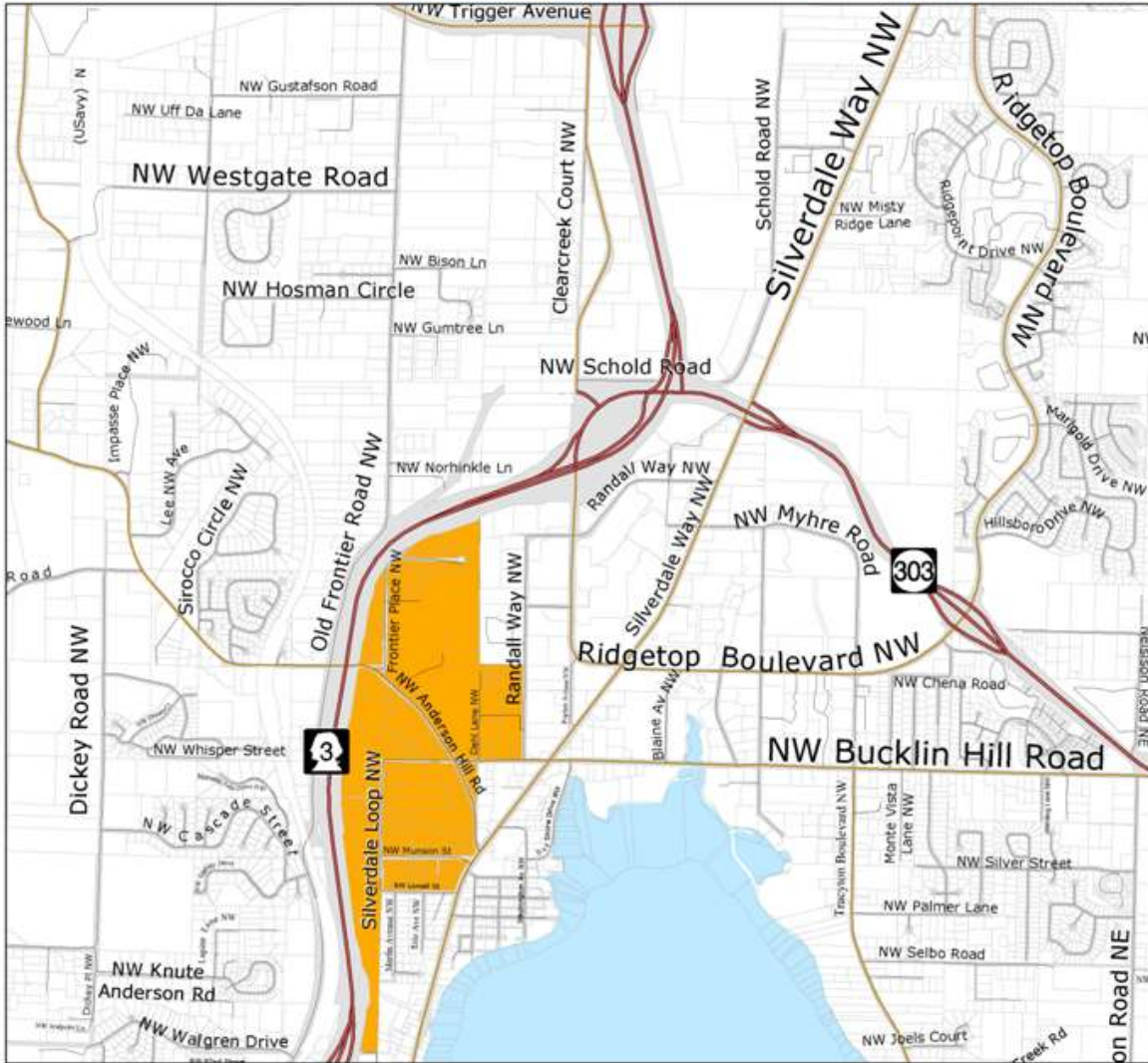
6.4.4 STREETS & PARKING

See the Bucklin Hill Center Design District Guidelines, Sections 4.3 and 4.4.2.

For buildings outside of the Kitsap Mall boundaries:

- A. Mixed-use and town or village centers will apply the same guidelines as outlined in the Bucklin Hill Center Design District, Chapter 4.
- B. New, remodeled or reconfigured buildings in the RC zone will have a maximum base height of 35 feet, with an approved height increase not to exceed 65 feet. Commercial buildings will have a transparent front façade on the ground floor facing the main street, which should compose a minimum of 50% of the width of the front façade.
- C. Commercial buildings will have main pedestrian entry facing front yard or main street.
- D. Commercial buildings will have a landscaped front yard setback of 20 feet with the following exception:
 - a. Front yard setbacks may be less than 20 feet if an outdoor or enclosed pedestrian space is provided along the front façade between the building and the public sidewalk, not to be less than 8 feet in depth, for use as outdoor dining, etc.
 - b. Commercial buildings facing onto a local street connector have no front or side yard setback requirements, but should have a transparent front façade facing that street not less than 50% of the front façade width.
- E. Storage facilities adjacent to residential areas will provide a decorative wall a minimum of 8 feet high or a landscaped minimum setback of 12 feet in width, with trees.

West Hill Neighborhood (WHN)



**Silverdale
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Figure 7-1

Chapter 7. West Hill Neighborhood

7.1 Physical Identity Elements & Opportunities

The neighborhood as a whole has views to the eastern wooded slopes and Dyes Inlet and is bounded by Highway 3 to the west and north. A wooded slope runs north/south generally along the west side of Silverdale Way and Randall Way.

Currently, academic facilities share the hill with residential uses to the north of Strawberry Creek and Silverdale Loop Road. Medium density housing exists along the eastern edge and slope of the neighborhood.

The neighborhood's natural and physical features include Strawberry Creek and a forested ravine between Munson Street to the south, Silverdale Way on the west, Silverdale Loop Road on the north and Anderson Hill Road on the west.

7.2 Design Intent

The West Hill Neighborhood will provide opportunities for mixed-use development, with new infill that respects and protects the Strawberry Creek ravine and wooded slope characteristics.

7.3 Design Principles

- A. Residential neighborhood and commercial characteristics comprised of landscaped front yards, parking to the rear or side yards shall be continued throughout the area regardless of scale of development.
- B. Developments will be multi-family or mixed-use in nature, compatible with the underlying zoning.
- C. Wooded slopes, ravines and surface water drainage areas will be protected and incorporated into new developments as open space features.
- D. Compact, clustered and attached housing developments are preferred for new housing.
- E. "Mixed density" design, where different building types are mixed within each cluster, compound or development phase, is preferred for new housing.
- F. Commercial and office activities should be designed to complement local neighborhoods and educational facilities.

7.4 Design Actions

7.4.1 RESIDENTIAL BUILDING ORIENTATION, HEIGHTS & TYPES

- A. New, remodeled or reconfigured buildings in the MU zone will have a maximum base height of 65 feet except where authorized by Kitsap County Code. In the RC and UH zones, the maximum base height will be 35 feet, with an approved height increase not to exceed 65 feet except where authorized by Kitsap County Code. In the UL zone, the maximum base height will be 35 feet, with no further height increase allowed.

-
- B. Mixed-density developments shall share parking facilities, access drives, and common open space and should vary the building type, using semi-attached and attached units within the development to gain more open space.

7.4.2 RESIDENTIAL OPEN SPACE & PEDESTRIAN CIRCULATION

- A. All buildings will have a landscaped front yard (hard and/or soft materials) with major entries oriented to and accessible from a front yard.
- B. “Front yard” may be defined as an entry courtyard, court, deck, porch, foyer or other foreground feature that contains the major entry(s) to building(s).
- C. All new multiple family developments will have a minimum of 64 square feet of designated private open space for each unit, in addition to any common open space areas. Required private open space may include individual balconies, decks or patios.
- D. Live/work buildings may accommodate some or all of the required residential outdoor open space on balconies or decks. Figures 7-2 *Live/Work Street Scene* and 7-3 *Live/Work Development with Street Orientation* show examples of a live/work development.
- E. All new development will have pedestrian sidewalk connections from front entry to public sidewalk, either from individual units, a collector sidewalk or courtyards.

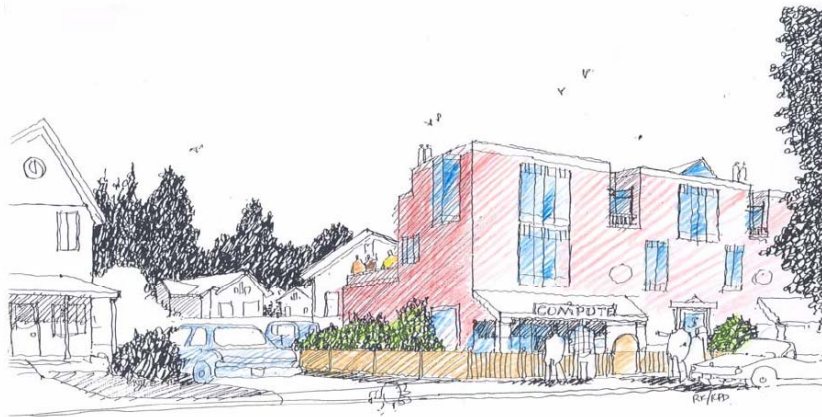


Figure 7-2 Live/Work Street Scene

7.4.3 RESIDENTIAL STREETS & PARKING

- A. Parking will be to the rear or side yards.
- B. Ground level accessory dwelling units (ADUs): one per building exempt from parking requirement, remainder treated as studio units.
- C. Parking access will be from alley, where feasible.
- D. Parking will be under building, where feasible, for multiple unit buildings.
- E. Driveways may be shared between and among different developments and property owners, with a minimum 18 feet wide access drive.



Figure 7-3 Live/Work Development with Street Orientation

7.4.4 COMMERCIAL BUILDING ORIENTATION, HEIGHTS & TYPES

- A. Mixed-use and town or village centers will apply the same guidelines as outlined in the Bucklin Hill Center Design District, Chapter 4.
- B. New, remodeled or reconfigured buildings in the MU zone will have a maximum base height of 65 feet. In the RC and UH zones, the maximum base height will be 35 feet, with an approved height increase not to exceed 65 feet except where authorized by Kitsap County Code. In the UL zone, the maximum base height will be 35 feet, with no further height increase allowed.
- C. Commercial buildings will have a transparent front façade on the ground floor facing the main street, which shall compose a minimum of 50% of the width of the front façade.
- D. Commercial buildings will have the main pedestrian entry facing the front yard/"main street" or a 12 foot wide pedestrian walkway in the side yard that connects to the "main street."
- E. Storage facilities adjacent to residential areas will provide 1) a decorative wall a minimum of 6 feet high, or 2) a minimum landscaped setback of 8 feet in width with a double row of trees or 3) hedges or other plantings at least 4 feet high and deep.
- F. Weather protection features such as canopies and marquees shall be provided on storefront buildings along major pedestrian walkways.

7.4.5 COMMERCIAL OPEN SPACE & PEDESTRIAN CIRCULATION

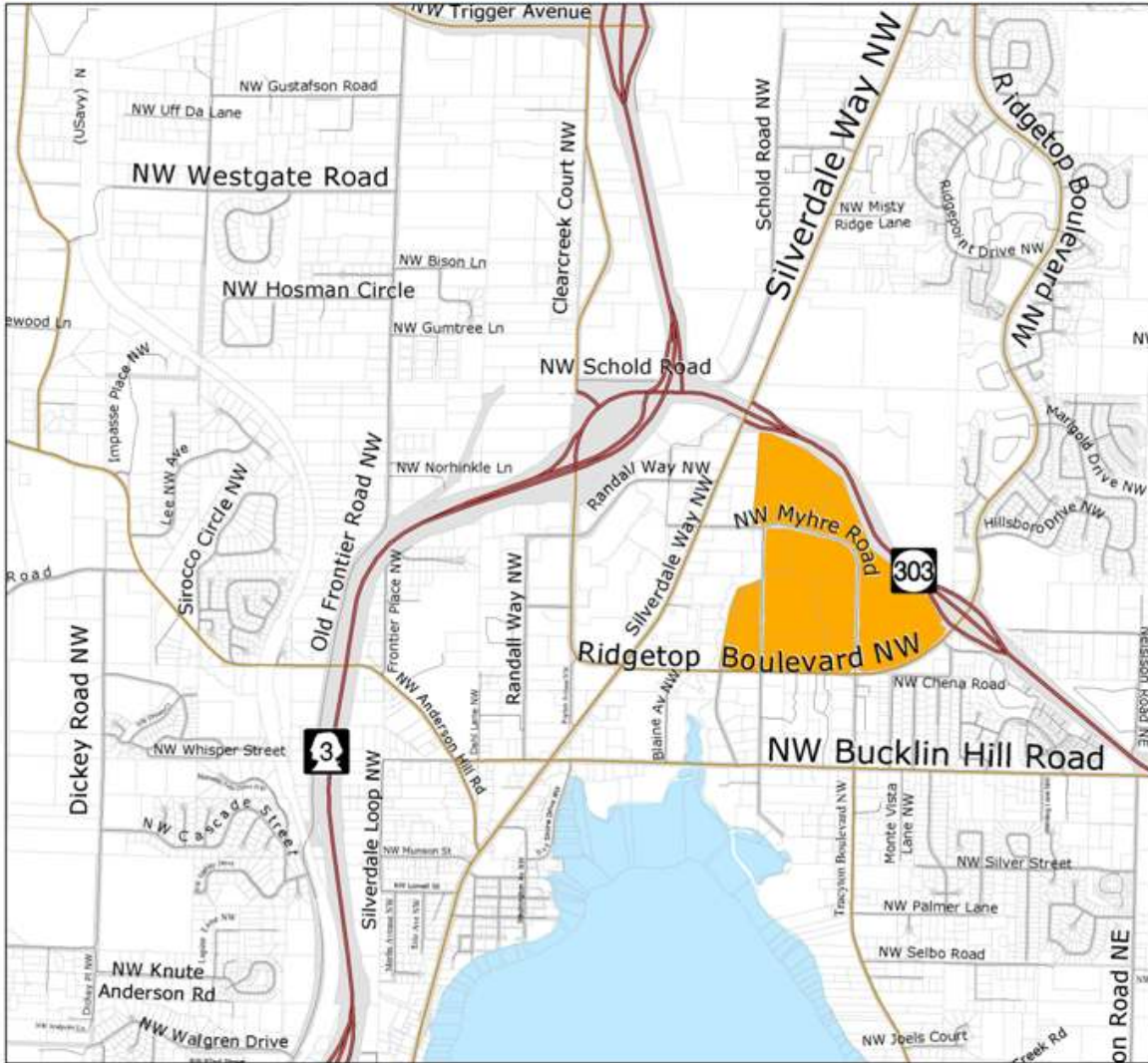
- A. Commercial buildings will have a landscaped front yard setback of 20 feet along "main street" with the following exceptions:
 - a. Front yard setbacks may be less than 20 feet if an outdoor or enclosed pedestrian space is provided along the front façade between the building and the public sidewalk, not less than 8 feet deep, for use as outdoor dining, etc., and absent any parking in front of building.
 - b. Commercial buildings facing onto a local street connector have no front or side yard setback requirements, but should have a transparent front façade (facing that street) not less than 50% of the front façade width.
 - c. Any front yard setbacks will be for pedestrian and/or landscape functions.

-
- B. Walkways will connect all front entries to the main street sidewalk network.
 - C. A continuous, paved walkway network, 8 feet wide minimum, will provide pedestrian circulation throughout the West Hill Neighborhood, connecting local street networks to arterials.

7.4.6 COMMERCIAL STREETS & PARKING

- A. “Main streets” and collector street(s) connecting the development to major arterials will be 22 to 24 feet in width with parallel parking places on one or both sides of street, with a raised or protected sidewalk (bollards, planting features) on both sides of street.
- B. Secondary streets connecting parking lots to main streets and collector streets will be 20 to 22 feet in width with a raised or protected sidewalk (bollards, planting features) and a parking lane on at least one side of street.
- C. Local streets that serve as delivery lanes may use “main street” dimensions.
- D. Parking areas will be small clusters of lots consisting of one to two double-loaded parking bays maximum, i.e., one driving lane flanked by one row of parking stalls on each side.
- E. On-street parking may be used to fulfill parking requirements for a proposed use if located on the subject parcel, contiguous with the public right-of-way and meeting all sidewalk and landscape requirements.
- F. Individual parking clusters shall be separated by an 8 feet wide minimum sidewalk protected by curbs, plantings or artistic bollards.

Northeast Business Park (NEB)



Silverdale Design District



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Figure 8-1

Chapter 8. Northeast Business District

8.1 Physical Identity Elements & Opportunities

The Northeast Business District includes Clear Creek and associated riparian area and wetlands. The district has views of Dyes Inlet and encompasses lands to the south of Highway 303. The predominant development pattern is office park, commercial and “big box” retail between Ridgetop Boulevard and Highway 303, with some high-density housing to the east.

8.2 Design Intent

Northeast Business District will be a campus type development pattern for office, office/retail, institutional and related uses. The design district establishes a *business park in a forest* setting, incorporating features of the forest and wooded slopes into development patterns as physical identity elements.

A “business park” is a development type with office, institutional, research-development, and live/work type buildings set in a *park-like* setting, with larger landscaped entry plazas, landscaped boulevards and landscaped parking lots. A “campus (office) park” type development pattern differs from a business park by the way buildings are arranged. In addition to a park-like setting, the campus type uses formal to semi-formal geometric shaped landscaped open spaces around which buildings are clustered. The park-like setting is incorporated into open space types such as parking lots, service and storage areas, and landscaped boulevards.

8.3 Design Principles

- A. The Northeast Business District buildings shall be located around and oriented to landscaped formal, semi-formal and natural open spaces, referred to as “campus quadrangles” – a conventional typology used on university campuses to organize and structure building arrangements, open space and parking.
- B. The Northeast Business District will be a pedestrian-friendly environment with convenient walkways connecting buildings and parking lots in a landscaped setting.
- C. Main pedestrian entrances to buildings shall be accessed from the “campus quadrangles” with pedestrian paths and walkways connecting quadrangles to parking lots and other quadrangle building clusters.
- D. Parking lots will be conveniently located in dispersed lots, avoiding large expansive areas of paved surfaces.
- E. Visitor parking should be visually evident from access roads, located in small landscaped lots close to buildings and separate from peripheral employee parking.
- F. Semi-formal arrangements of open space geometries will respond to and adapt to the sloping wooded slopes, and should retain trees and natural features
- G. Access roads will continue the formal landscape characteristics of the open spaces with landscaped edges or medians.
- H. Business Park development patterns will protect and buffer adjacent residential developments with special landscape features.

8.4 Design Actions

8.4.1 OFFICE/CAMPUS COMPOSITIONAL STRUCTURE

- A. The Northeast Business Park shall be composed of interior landscaped quadrangles that are pedestrian oriented, bordered by buildings and dispersed parking lots, with clusters of quadrangles connected to one another by landscaped promenades.

8.4.2 BUILDING ORIENTATION, HEIGHTS & TYPES

Campus Office Parks

- A. For campus office parks, buildings will be oriented to campus quadrangles where the front (and entry) yard is a formal or semi-formal open space, with parking and service access dispersed throughout the development. Figure 8-2 *Multiple Quadrangle Combinations* illustrates one way to site buildings in this fashion.
- B. New, remodeled or reconfigured buildings in the RC zone will have a maximum base height of 35 feet, with an approved height increase not to exceed 65 feet except where authorized by Kitsap County Code.
- C. Building types may include double-loaded corridor buildings, central circulation core buildings, and townhouse over flats for live/work.
- D. Access drives with emergency vehicle access and drop-off/pick-up cutouts may be provided within pedestrian-oriented campus quadrangles.

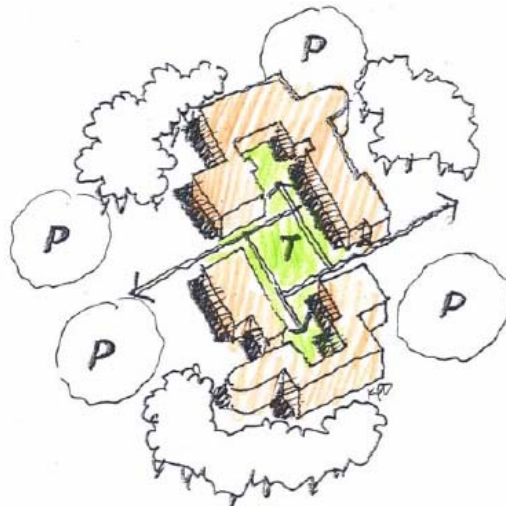


Figure 8-2 Multiple Quadrangle Combinations

Retail/Office Parks

- A. For office/retail parks where employee populations are smaller and truck/van access is needed, buildings may be oriented to landscaped front yards without campus style shapes, with parking and service access located within the interior or rear of building arrangements. See Figure 8-3 *Warehouse Park Diagram*.
- B. New, remodeled or reconfigured buildings in the RC zone will have a maximum base height of 35 feet, with an approved height increase not to exceed 65 feet. Flat roofs are acceptable.

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- C. Live/work buildings may be townhouse-type buildings above ground level flats or flats stacked above ground level office/warehouse uses.

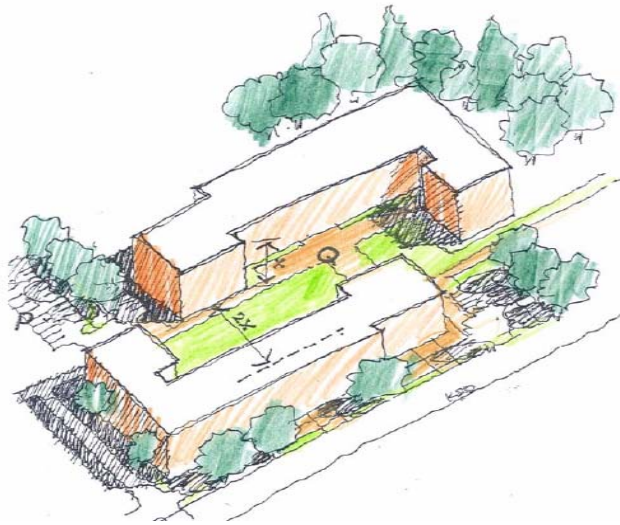


Figure 8-3 Warehouse Park Diagram

8.4.3 OFFICE/CAMPUS OPEN SPACE FEATURES

- A. Widths of campus quadrangles (diameters, widest points, etc.) shall be twice (2h) the height (h) of the highest building measured on building face closest to the quadrangle; for example, a 65 high office building has a 130 wide quadrangle as its front yard.
- B. Campus quadrangles and park open spaces will have landscaping features including retained existing trees, ornamental trees and shrubs, and sculpture where feasible.
- C. Pedestrian promenades that connect one quadrangle cluster to another shall be a minimum of 20 feet in width with landscape features separating pedestrian walkways, paths, trails from vehicular lanes using landscaped mounds, shrubbery, hedges or landscape walls not to exceed 3 feet in height.
- D. Water features should incorporate surface water filtration and retention aspects into their function and design.
- E. Wetlands and areas adjacent to Clear Creek will be used as natural campus open space features where feasible.

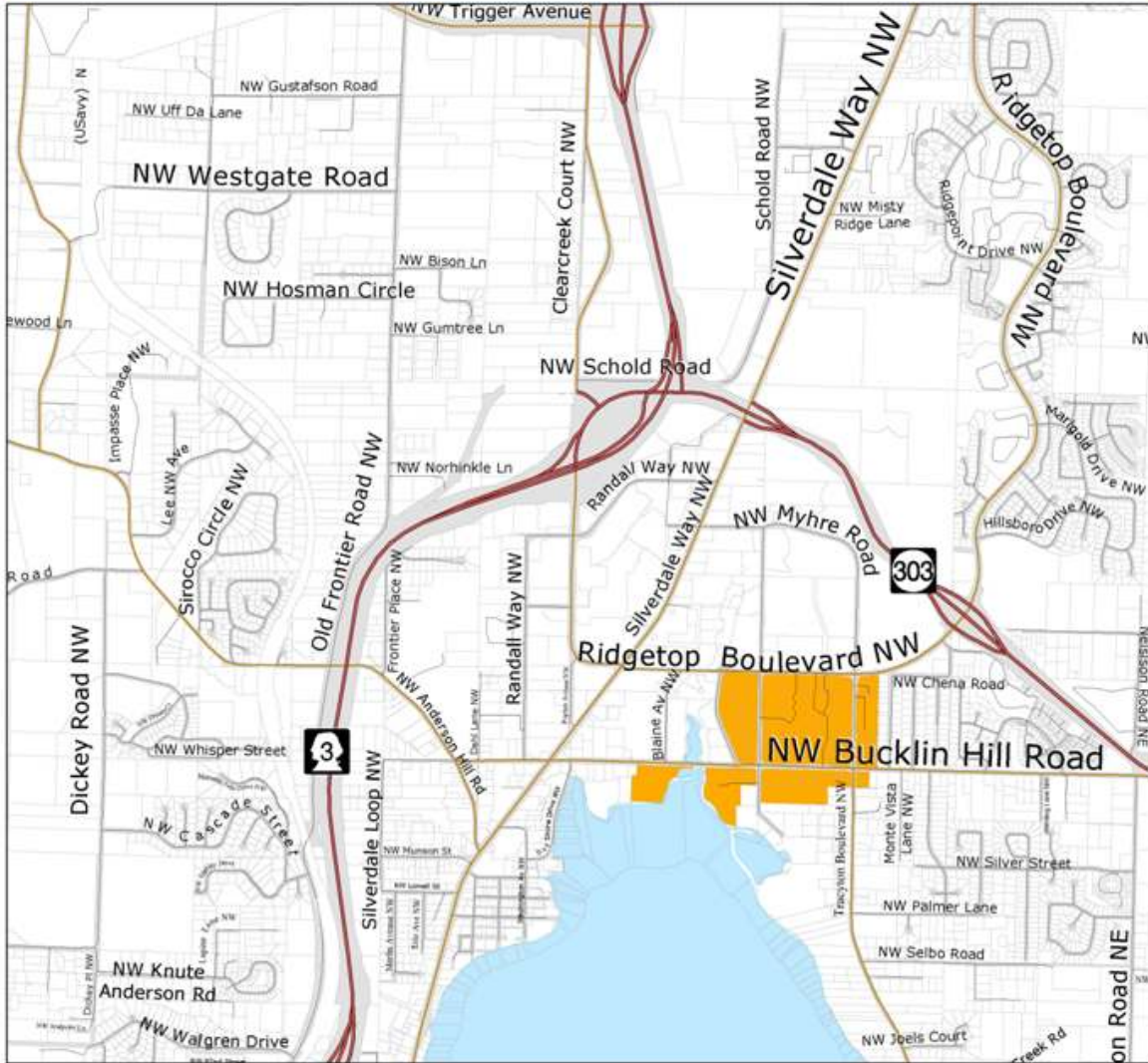
8.4.4 OFFICE/CAMPUS VEHICULAR CIRCULATION & PARKING

- A. Access roads will have a hierarchy of types including a primary entry street with landscaped edges, sidewalks and landscaped median, and 10 to 12 feet wide lanes in each direction plus turning lanes, with cutouts for drop-off/pick-up not to exceed three car lengths.
- B. Pedestrian crosswalks, raised or emphasized with contrasting materials, will be provided at key connections between parking lots and sidewalk network.
- C. Surface parking lots will be limited to two rows of stalls served by a single access drive and will be separated from additional parking lots by landscape buffers at

least 8 feet in width, containing shrubs, hedges, grassy mounds or other suitable vegetated treatments.

- D. All parking lots will have separated sidewalks, 6 feet wide minimum, connecting them to major pedestrian and open space areas.

Waterfront (W)



Silverdale Design District



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Figure 9-1

Chapter 9. Waterfront District

9.1 Physical Identity Elements & Opportunities

The Waterfront District is a mixture of established multi and single-family homes south of Bucklin Hill Road and east of Mickelberry Road and commercial development north of Bucklin Hill Road. Properties adjacent to Bucklin Hill Road and near Dyes Inlet enjoy partial views of Dyes Inlet and the Clear Creek corridor.

9.2 Design Intent

Multi-family residential will be maintained near to and north of Bucklin Hill Road and east of Mickelberry Road and will require buffering from non-residential uses. New and reconfigured commercial developments will provide landscaped buffers for residential areas, and will provide more open space.

9.3 Design Principles

- A. Extend pedestrian walkways along the Dyes Inlet shoreline, connecting them to Bucklin Hill Road and Tracyton Boulevard.
- B. New design will be respectful of existing single family residential development.
- C. Parking shall be located in side or rear yards, with alley access.
- D. Commercial buildings will access arterials with shared driveways or from local street network.
- E. Commercial buildings will have a landscaped front yard, with preferred location of parking in rear or on side.

9.4 Design Actions

9.4.1 RESIDENTIAL BUILDING ORIENTATION, HEIGHTS & TYPES

- A. Building orientation for new housing will reflect the existing residential building context, with buildings and main entries facing front yards or from courtyards, decks, or plazas that relate to front yards.
- B. New, remodeled or reconfigured buildings in the UH zone will have a maximum base height of 35 feet, with an approved height increase not to exceed 65 feet except where authorized by Kitsap County Code.
- C. Mixed-density developments will share parking facilities, access drives, common open space, and will vary the building type within the development to gain more open space.
- D. Weather protection features such as canopies and marquees shall be provided on buildings along major pedestrian walkways.

9.4.2 RESIDENTIAL OPEN SPACE & PEDESTRIAN CIRCULATION

- A. All residential buildings will have a landscaped front yard (hard or soft materials) with major entries oriented to and accessible from a front yard.
- B. “Front yard” may be defined as an entry courtyard, court, deck, porch, foyer or other foreground feature that contains the major entry(s) to building(s).
- C. All new townhouse, row house, cottage, courtyard, and other cluster housing with one or two stories will have a minimum of 64 square feet of designated, private,

ground level open space for each unit, in addition to any common open space areas.

- D. Live/work buildings may accommodate some or all of the required residential outdoor open space on balconies or decks.
- E. All new developments will have pedestrian sidewalk connections from front entry to public sidewalk, either from individual units, a collector sidewalk, or courtyards.

9.4.3 RESIDENTIAL STREETS & PARKING

- A. Parking will be limited to the rear or side yards.
- B. Ground level accessory dwelling units (ADUs): one per building exempt from parking requirement, remainder treated as studio units.
- C. Parking access will be from alley where feasible.
- D. Parking will be under building where feasible for multiple unit buildings.
- E. Driveways may be shared between and among different developments and property owners, with a minimum of 18 feet wide driveways on the same property and 20 feet wide driveways shared with two or more properties.

9.4.4 COMMERCIAL BUILDING ORIENTATION, HEIGHTS & TYPE

- A. Mixed-use and town or village centers will apply the same guidelines as outlined in the Bucklin Hill Center Design District, Chapter 4.
- B. New, remodeled or reconfigured buildings in the RC zone will have a maximum base height of 35 feet, with an approved height increase not to exceed 65 feet except where allowed by Kitsap County Code. Commercial buildings will have a transparent ground level front façade facing the main street, which should be a minimum of 50% of the width of the front façade.
- C. Commercial buildings will have the main pedestrian entry facing the front yard/"main street" or a 12 feet wide pedestrian walkway in the side yard, that connects to the "main street."
- D. Storage facilities adjacent to residential areas will provide 1) a decorative wall a minimum of 6 feet high, or 2) a minimum landscaped setback 8 feet wide with a double row of trees, or 3) hedges or other plantings at least 4 feet high and deep.
- E. Weather protection features such as canopies and marquees shall be provided on storefront buildings along major pedestrian walkways.

9.4.5 COMMERCIAL OPEN SPACE & PEDESTRIAN CIRCULATION

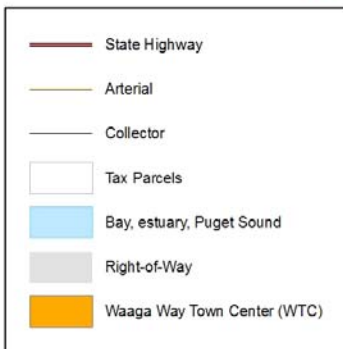
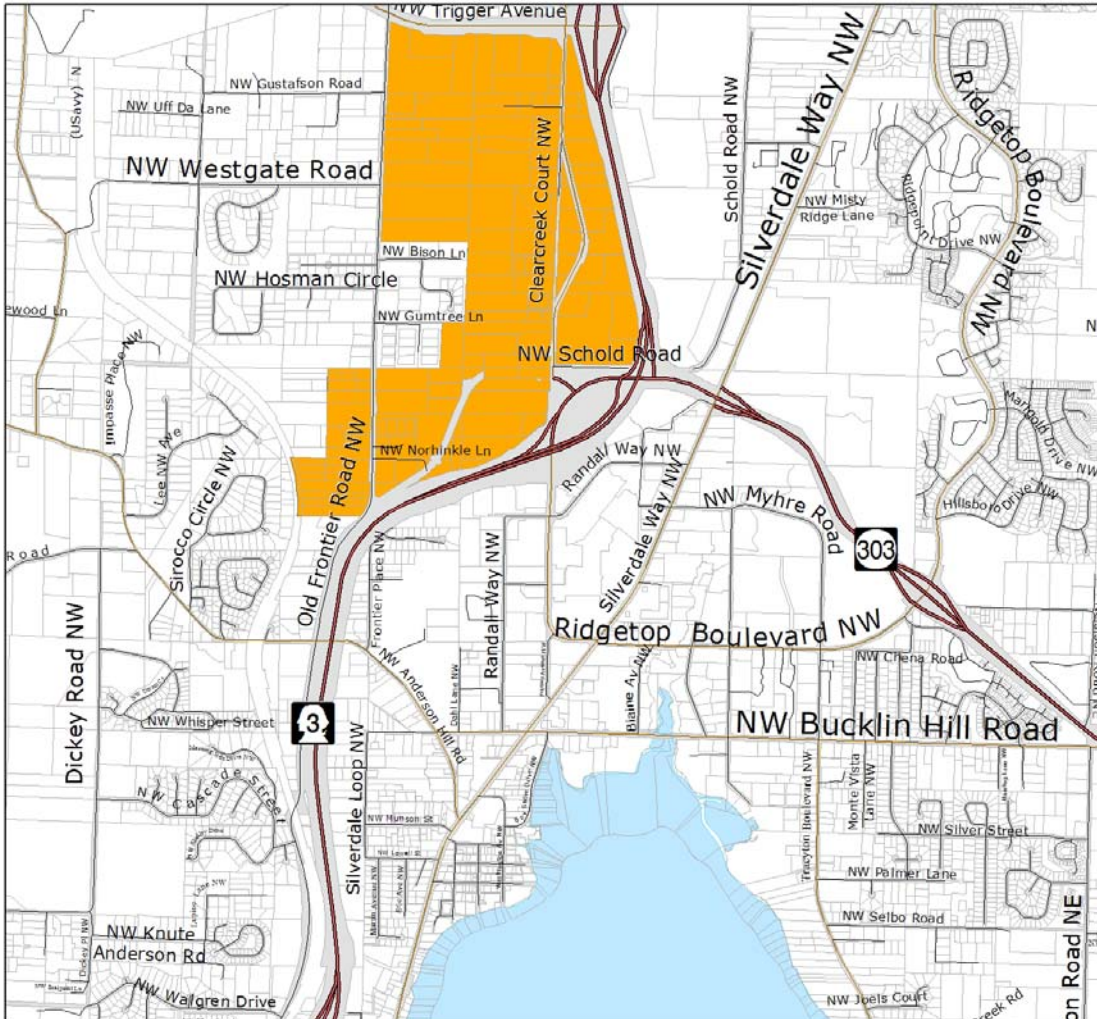
- A. Commercial buildings will have a landscaped front yard setback of 20 feet along all major arterials with the following exceptions:
 - a. Front yard setbacks may be less than 20 feet if an outdoor or enclosed pedestrian space is provided along the front façade between the building and the public sidewalk, not less than 8 feet deep, for use as outdoor dining, etc.
 - b. Commercial buildings facing onto a local street connector will have no front or side yard setback requirements, but should have a transparent front façade (facing that street) not less than 50% of the front façade width.

-
- c. Any front yard setbacks will be for pedestrian and/or landscape functions.
 - B. Walkways will connect all front entries to the main street sidewalk network.
 - C. A continuous, paved walkway network, 8 feet minimum width, will provide pedestrian circulation throughout the Waterfront District, connecting local street networks to arterials and the waterfront walkway and parks.
 - D. A continuous waterfront walkway will provide public access to Dyes Inlet with connections to Bucklin Hill Road, Tracyton Boulevard and waterfront parks.

9.4.6 COMMERCIAL STREETS & PARKING

- A. Parking for commercial buildings shall be located in side or rear yards.
- B. Parking lots will be arranged in double bay clusters, each bay a double loaded lot with a row of parking on each side of an access drive, each bay separated by a 6 feet wide landscaped island or a raised sidewalk and planting area combination at least 8 feet wide.
- C. On-street parking may be used to fulfill parking requirements for a proposed use if located on the subject parcel, contiguous with the public right-of-way and meeting all sidewalk and landscape requirements.
- D. A local street network will connect new commercial and residential developments to perimeter arterials
- E. Local street network will have sidewalks 8 feet wide minimum, separated from vehicular traffic where possible by landscaped buffers at least 4 feet wide.

Waaga Way Town Center (WTC)



**Silverdale
Design District**

May 2009



Kitsap County Department of
Community Development
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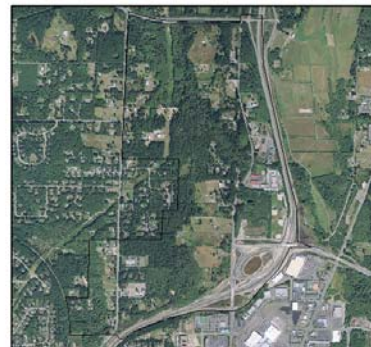


Figure 10-1

Chapter 10. Waaga Way Town Center

10.1 Physical Identity Elements & Opportunities

The Waaga Way Town Center includes land that is largely undeveloped and underutilized. The district is located northwest of the Highway 3/303 interchange in Silverdale. The predominant development pattern is single family residences with existing commercial development along Provost, Old Frontier and Clear Creek Roads.

The neighborhood's natural and physical features include large forested areas, isolated wetland systems and moderate to steep slopes with views of downtown Silverdale and Dyes Inlet.

10.2 Design Intent

Waaga Way Town Center is intended to include a compatible mix of office/retail, business and residential uses. Developments in this area should be constructed in a coordinated park-like setting using landscaping and open spaces where appropriate. Businesses should be oriented toward pedestrian and bicycle pathways, transit facilities, recreational trails and gathering areas. Such development shall include an integrated system of pedestrian and bicycle pathways to ensure their compatibility with vehicular traffic in the area. Architecture shall be distinct and draw focus to business entrances and intersections with public streets and interior access roads.

The following standards and guidelines are intended to implement the goals and policies of the Silverdale Sub-Area Plan and the purpose of the commercial, industrial, business center and urban low residential (applies to urban low only when specifically noted) zoning districts (Kitsap County Title 17). The standards are a mandatory requirement and are intended to be used in conjunction with the requirements of Kitsap County Code. Certain guidelines are general in their nature in order to encourage unique and innovative site design solutions from the applicant and to provide a varied landscape in the county.

10.3 Design Principles

- A. The Waaga Way Town Center shall be a pedestrian and bicycle-friendly environment built to a human-scale with convenient pathways, connecting buildings and parking lots in a landscaped setting.
- B. Parking shall be located in dispersed lots, while avoiding large expansive areas of paved surfaces, where feasible. Parking areas shall include landscaped pedestrian and bicycle pathways that are protected from vehicular traffic. Development shall include low impact development techniques, where feasible.
- C. Future development should avoid disturbance of the natural topography by retaining trees and natural features, where feasible. Development shall also incorporate natural vistas, such as views to Dyes Inlet, the Cascade Mountain range, and Clear Creek into the overall orientation of the development.
- D. Interior access roads shall provide landscaped edges or medians.

-
- E. Larger buildings shall be designed in a manner to have the exterior appearance from all public areas of several smaller buildings using architectural features or other methods.
 - F. Buildings shall be architecturally distinct using multiple roof forms and elevations. Building scale and bulk shall be mitigated through the use of prominent architecture at corners, focal points and business accesses. The visual effect of expansive walls shall be broken up using wall treatments and/or horizontal articulation.
 - G. Business center, industrial and commercial development patterns will protect and buffer adjacent residential developments with enhanced landscape features and other techniques.
 - H. Development shall use innovative and sustainable green building techniques, where feasible. Such techniques may include, but not limited to, energy-efficient construction, green roofs, and/or solar or geothermal energy production.
 - I. Development adjacent to the Clear Creek Trail system shall be coordinated with these facilities with private open spaces and gathering places oriented towards them.

10.4 Waaga Way Connector Roads

The Silverdale Sub-Area Plan, adopted on December 11, 2006 and amended on December 23, 2008, provides policy guidance on roadway design and location of connector roads to the Waaga Way Extension Road. Generally, these connector roads are intended to provide a coordinated circulation pattern within the Waaga Way Town Center. These connector roads are classified as local sub-collectors in the Kitsap County Road Standards and shall be located consistent with Figure 10.1 (shown in blue).

The northern connector roads are identified as those north of the Waaga Way Extension Road, between Old Frontier and Clear Creek Roads. These northern roadways shall be designed, at minimum, consistent with Figure 10.2. The southern connector roads are identified as those south of the Waaga Way Extension Road and north of State Route 3 and, shall be designed, at minimum, as shown Figure 10.3. Road names of these connector roads shall be determined by the Kitsap County Board of Commissioners.

While allowed to be constructed at one time, it is likely that construction of these connector roads (funded by private development) would occur in two phases. Phase One (blue lines) would likely occur serving the commercial properties located nearest the Extension Road. This Phase would depend largely on the Extension Road for traffic circulation with no required connections to Old Frontier or Clear Creek Roads. The connection between the cul-de-sac and Old Frontier Road (Phase Two) shall be determined through a future planning process.



Figure 10.2 Connector Road Alignment

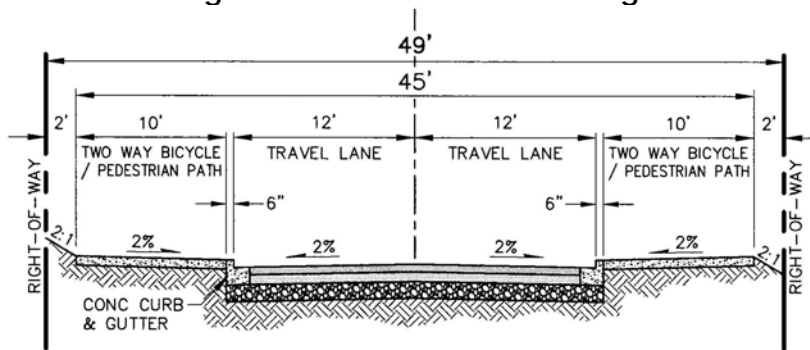


Figure 10.3 Northern Connector Road Design

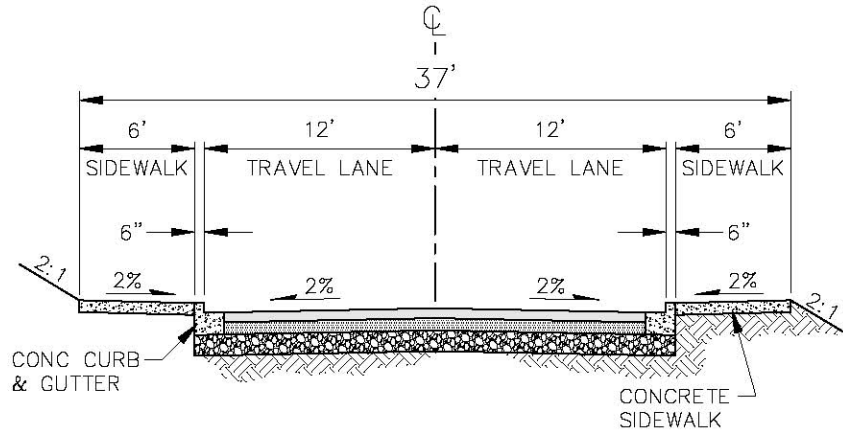


Figure 10.4 Southern Connector Road Design

Additional modifications to these locations and designs may be allowed with the aggregation of property ownership or project engineering features if such modifications maintain the intent of free-flowing, compatible design and multi-modal access to the Waaga Way Extension Road.

10.5 Design Actions

10.5.1 SITE DEVELOPMENT STANDARDS

- A. To provide for a welcoming and accessible streetscape, primary building entrances shall be oriented toward pedestrian, bicycle and transit facilities.
- B. Buildings within the Regional Commercial, Highway Tourist Commercial and Business Center zones shall be located as close as possible to the street and sidewalk. Such buildings located nearest the Waaga Way Extension Road, connector roads or other public streets shall be setback a maximum of twenty-five (25) feet from these right-of-ways. No parking areas are permitted within this setback or between these roadways and any proposed buildings. For buildings not located at the street, the circulation drive and parking at the front of the building shall be designed with the look and feel of a street including, but not limited to street trees, street furniture, and/or sidewalks.
- C. Commercial development should provide spaces for civic interaction and frequently serve as public spaces. To make these more accessible and accommodating to the public, pedestrian plazas, street furniture and public open spaces shall be incorporated as site amenities whenever possible. The location and design of these spaces shall be integrated into the overall project design.
- D. To create welcoming public spaces, establish character and increase accessibility and access to development, buildings on corner lots shall be located on the street corner with building frontage and prominent architectural design on both streets.-
- E. If buildings are not located at the corner intersections, monumentation shall be incorporated at key or focal intersections within or on the perimeter of developments to foster a sense of community identity and arrival. Monumentation shall be of an appropriate size, scale and design to accomplish this intent. Signage may be

considered such monumentation if consistent with a master sign district regulations of KCC Chapter 17.445.

10.5.2 ARCHITECTURAL AND BUILDING DESIGN AND HEIGHT STANDARDS

- A. Building walls located along the Waaga Way Extension Road, connector roads or other public streets shall be horizontally articulated a minimum of five (5) feet every seventy-five (75) feet of their length.
- B. Any building wall greater than fifty (50) feet in length shall include one (1) or more of the following elements:
 - 1. Transparent windows or doors.
 - 2. Architectural treatments such as mosaic, glass block or similar features.
 - 3. Trellis with climbing plants.
 - 4. Display windows.
 - 5. Other features the interrupt blank walls.
- C. All buildings located nearest a full-motion access intersection with a connector road and/or the Waaga Way Extension Road shall include prominent architecture at the corner facing the intersection.
- D. Uses greater than 10,000 square feet in size shall include prominent architecture at their entrance.
- E. Buildings less than or equal to 10,000 square feet in size shall include a minimum of two roof/façade elevations.
- F. Buildings between 10,000 and 25,000 square feet in size shall include a minimum of three roof/façade elevations.
- G. Buildings 25,000 square feet or greater in size shall include a minimum of four roof/façade elevations.
- H. Buildings shall integrate awnings, overhangs or other rain protection features when abutting pedestrian amenities.
- I. Height shall be consistent with the requirements of Kitsap County Code.

10.5.4 VEHICULAR CIRCULATION & PARKING STANDARDS

- A. Parking lots shall be integrated in such a manner as to encourage pedestrian and bicycle access to the development, connect uses to the street and decrease the distance between adjacent developments.
- B. The multi-modal circulation system and parking shall be designed as an extension of the street system to provide a connection to the street and to draw users onto the site. This shall be accomplished through the use of continuous sidewalks, bicycle pathways, landscaping, utilization of the street elevation, the use of parallel and/or angle parking and the use of driveways and circulation drives as mid-block connections, where feasible.
- C. In order to reduce the amount of land devoted to surface parking, shared parking between users is encouraged. Users with differing peak periods such as offices and movie theaters are especially suitable for shared parking. Shared parking may count for up to 40% of required minimum number of spaces.

-
- D. Common driveways between users are strongly encouraged wherever possible in order to reduce curb cuts and the frequency of automobile traffic interrupting pedestrian and bicycle infrastructure. All curb cuts shall be as narrow as safety allows in order to reduce the distance pedestrians must cross.
 - E. To provide greater opportunity for pedestrian and bicycle interconnectivity and to prevent automobiles from having to use the public street system to travel between adjacent developments, parking and pedestrian and bicycle circulation shall be designed to accommodate connections between developments, where feasible.
 - F. For properties zoned Regional Commercial, Highway Tourist Commercial, Urban Low Residential or Business Center, a minimum of a 15-foot landscaping buffer is required along the Waaga Way Extension Road, all connector roads and other public streets. Vegetation shall include street trees and shrubbery. Irrigation may be required based upon the species of vegetation installed. The uses allowed within this buffer include utilities, street lighting, gathering areas, pedestrian and bicycle access, transit facilities and vehicular access to the overall development.
 - G. For properties zoned Industrial, a 25-foot screening buffer is required along the Waaga Way Extension Road, all connector roads and other public streets. Vegetation shall include evergreen trees and other vegetation to provide adequate functional screening of the uses on site.
 - H. Rows of parking stalls shall include a planting strip every ten (10) stalls protected by a vertical curb. Such a strip shall be five (5) feet in width and include a minimum of one (1) tree and other shrubbery. Irrigation may be required based upon the species of vegetation installed.
 - I. Parking stalls abutting a building frontage shall include an planting strip every six (6) stalls protected by a vertical curb. Such a strip shall be a minimum of five (5) feet in width and include a minimum of one (1) tree and other shrubbery. Irrigation may be required based upon the species of vegetation installed.
 - J. In order to reduce the growth of surface parking which results in decreased intensity of uses, increased sprawl and separation of uses and anti-pedestrian environments, the amount of surface parking shall be limited to no more than one hundred percent (100%) of the required minimum amount. Any parking in excess of the 100% required minimum must be located within a parking structure(s).

10.5.5 PEDESTRIAN INFRASTRUCTURE STANDARDS

- A. Pedestrian linkages shall be designed to provide the most direct route to the transportation infrastructure. Linkages should be a continuation of the infrastructure to reduce dead-end paths and encourage users to the development. Consideration shall be given to off-site destinations in the design of the system.
- ~~B.~~ Pedestrian and bicycle pathways shall be coordinated both on-site and within adjacent developments, where feasible. Such pathways are required through the site connecting buildings or parking lots. At minimum, one (1) pathway shall be located every two (2) aisles of parking stalls. Such pathways shall be a minimum of five (5) feet in open width if serving only pedestrian traffic and ten (10) feet in open width if serving both pedestrian and bicycle traffic. Such pathways shall be aligned with building entrances and, where feasible, and landscaped on both sides including a row of trees protected from vehicle traffic by a vertical curb. All pedestrian and bicycle

pathways and crossings of vehicle lanes shall be delineated with a distinct surfacing different from the driving areas in color, texture and composition.

- C. Amenities such as benches, sitting areas, art, pedestrian squares and varied building facades shall be provided along pedestrian routes to add interest and convenience to the user. These amenities shall share a common construction with the pedestrian and bicycle pathways in color, texture and composition.
- D. To provide for convenience, pedestrian and bicycle facilities along building frontages and developments shall incorporate rain protection and boulevard landscaping whenever possible. When such facilities are provided, special attention should be given to ensure safety, security and convenience by not creating enclosed spaces that may shelter potential criminal activity. Appropriate lighting shall be incorporated whenever possible.

10.6 INCORPORATION OF LOW-IMPACT DEVELOPMENT TECHNIQUES

Development within the Waaga Way Town Center shall utilize low-impact development techniques allowed by Kitsap County Code, where feasible. Through the incorporation of low-impact development techniques, site amenities such as landscaping and buffers may be designed and calculated as stormwater facilities.

10.7 MODIFICATIONS ALLOWED THROUGH SILVERDALE DESIGN COMMITTEE REVIEW

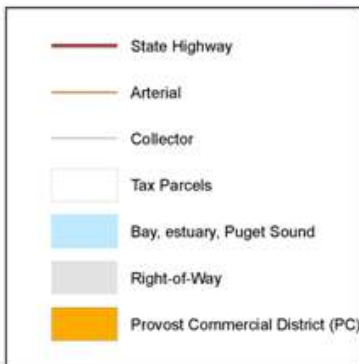
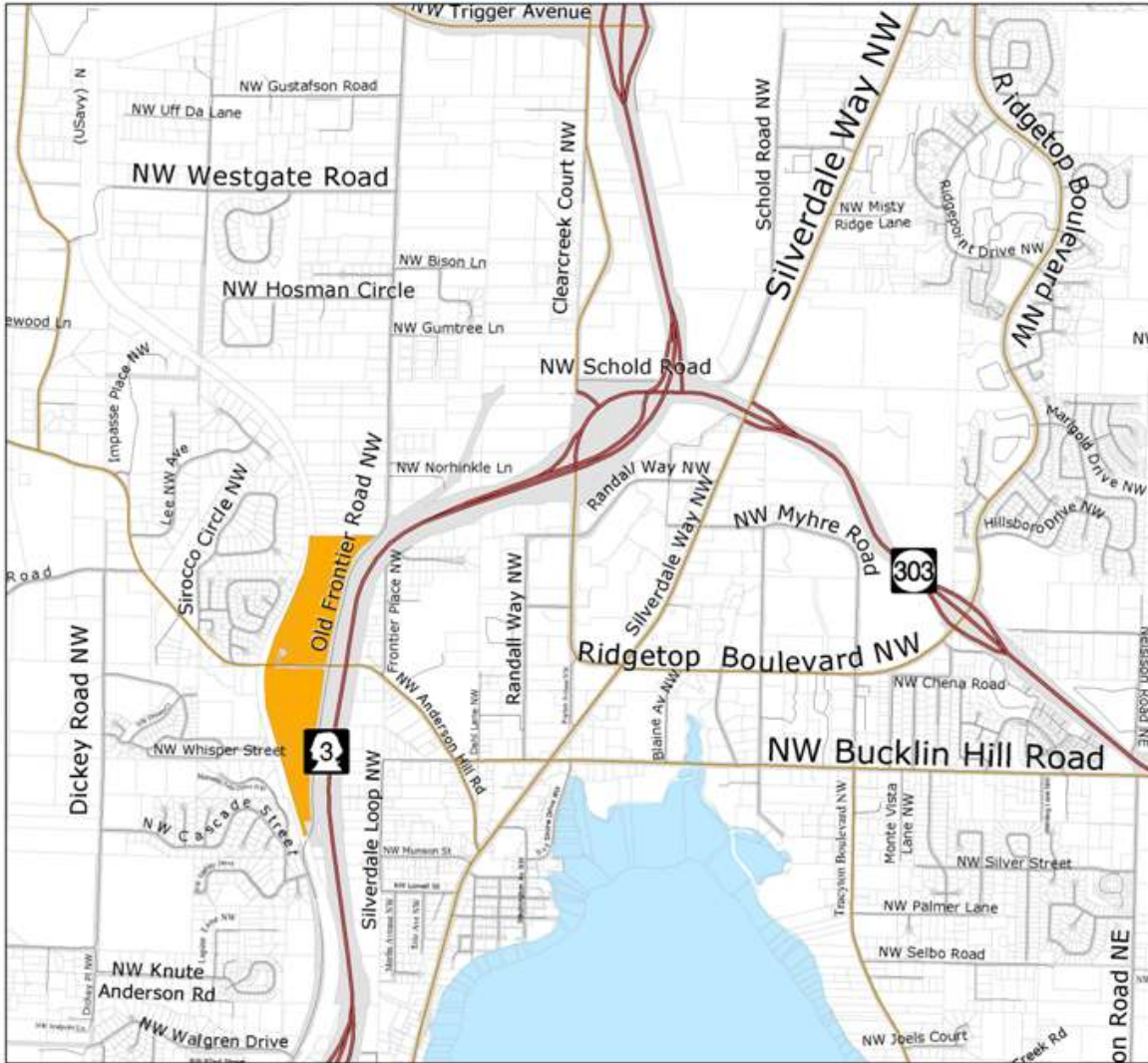
Once established by the County, applicants may request review by the Silverdale Design Committee to address site-specific project modifications to these standards. Such modifications may be requested to:

- a. Address site constraints such as topography or critical areas that make specific design standards infeasible in the area in which they are required; and/or
- b. Allow for innovative and sustainable development techniques.
- c. Allow for deviations from the standards set forth herein as long as sections 10.2 and 10.3 are achieved.

An applicant must show such modifications maintain the district's design intent and principles and are consistent with other applicable sections of Kitsap County Code Title 17 (Zoning).

After completion of review, the committee shall provide their written recommendations to the appropriate approval body as prescribed by Kitsap County Code Title 21 (Land Use Procedures).

Provost Commercial District (PC)



Silverdale Design District



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Figure 11-1

Chapter 11. Provost Commercial District

11.1 Physical Identity Elements & Opportunities

The Provost Commercial District is located across Highway 3 from the West Hill Neighborhood District. This district is encompassed completely with Highway Tourist Commercial (HTC) zoning. HTC zoning is intended to provide for commercial establishments which require large sites. This zone serves the shopping and service needs for large sections of the county and provides visitor services and accommodations for both destination and en route travelers. Mixed-use development is allowed within the HTC zone.

The neighborhood's natural and physical features include large forested areas and moderate to steep slopes with views of downtown Silverdale and Dyes Inlet.

11.2 Design Intent

The following standards and guidelines are intended to implement the goals and policies of the comprehensive plan and the purpose of the commercial zoning districts (Kitsap County Title 17). The standards are a mandatory requirement and are intended to be used in conjunction with the requirements of Kitsap County Title 17. The guidelines are general in their nature in order to encourage unique and innovative site design solutions from the applicant and to provide a varied landscape in the county.

The design intent of the Provost Commercial District is to coordinate development in a manner that results in an attractive streetscape, to promote traffic safety and to coordinate internal access.

11.3 Design Principles

- A. The Provost Commercial District will be a pedestrian-friendly environment with convenient walkways connecting buildings and parking lots in a landscaped setting.
- B. Parking lots will be conveniently located in dispersed lots, avoiding large expansive areas of paved surfaces. Low impact development is encouraged.
- C. Visitor parking shall be visually evident from access roads, located in small landscaped lots close to buildings and separate from peripheral employee parking.
- D. Semi-formal arrangements of open space geometries will respond to and adapt to the sloping wooded slopes, and should retain trees and natural features.
- E. Access roads will continue the formal landscape characteristics of the open spaces with landscaped edges or medians.
- F. Highway Tourist Commercial development patterns will protect and buffer adjacent residential developments with special landscape features.
- G. Wooded slopes, ravines and surface water drainage areas will be protected and incorporated into new developments as open space features.
- H. Commercial and office activities shall be designed to complement local neighborhoods and educational facilities.

11.4 Design Actions

11.4.1 SITE DEVELOPMENT GUIDELINES & HEIGHTS

- A. To provide for a welcoming and accessible streetscape, primary building entrances shall be oriented toward the public street and pedestrian and transit facilities.
- B. Buildings shall be located as close as possible to the public street and sidewalk, preferably at the sidewalk line or the minimum required buffer. If buildings are not located at the street, the circulation drive and parking at the front of the building shall be designed with the look and feel of a street including such things as street trees, parallel parking, street furniture, windows and sidewalks.
- C. Commercial development should provide spaces for civic interaction and frequently serve as public spaces. To make these more accessible and accommodating to the public, pedestrian plazas, street furniture and public open spaces should be incorporated as site amenities whenever possible. Creative configuration and type of landscaping and buffering can be used to accomplish this.
- D. To create welcoming public spaces and increase accessibility and penetrability of commercial development, buildings on corner lots shall be located on the street corner with building frontage on both streets with primary entrances oriented toward the intersection. If no buildings are located at street corners pedestrian plazas and amenities should provide a focus for the area.
- E. New, remodeled or reconfigured buildings in the HTC zone will have a maximum base height of 35 feet, with an approved height increase not to exceed 65 feet except where authorized by Kitsap County Code.

11.4.2 ARCHITECTURAL AND BUILDING DESIGN GUIDELINES

- A. The ground floor of building facades facing streets shall have at least fifty percent (50%) of the total wall area in permeable surfaces (windows, pedestrian entrances, open shops).
- B. Buildings with frontages greater than fifty (50) linear feet shall vary the building line and create offsetting walls, awnings, arcades, modulated wall textures, climbing landscaping materials and other features that break down the scale of the wall.
- C. Buildings shall integrate awnings, overhangs or other rain protection features when abutting pedestrian amenities.

11.4.3 COMMERCIAL VEHICULAR CIRCULATION & PARKING

- A. Parking lots should be located in such a manner as to encourage pedestrian access to the development, connect uses to the street and decrease the distance between adjacent developments. To accomplish this, parking shall be located behind and to the side of buildings and should not be placed on the corner of two streets.
- B. The auto circulation system and parking should be designed as an extension of the public street system to provide a connection to the public street and to draw users onto the site. This can be accomplished through the use of continuous sidewalks, landscaping consistent with public streets, utilization of the street elevation, the

-
- use of parallel parking and the use of driveways and circulation drives as mid-block connections.
- C. In order to reduce the amount of land devoted to surface parking, shared parking between users is encouraged. Users with differing peak periods such as offices and movie theaters are especially suitable for shared parking.
 - D. Common driveways between users are strongly encouraged wherever possible in order to reduce curb cuts and the frequency of auto traffic interrupting the pedestrian infrastructure. All curb cuts shall be as narrow as safety allows in order to reduce the distance a pedestrian must cross.
 - E. To provide greater opportunity for pedestrian interconnectivity and to prevent autos from having to use the public street system to travel between adjacent developments, parking and pedestrian circulation shall be designed to accommodate connections between developments.
 - F. To facilitate more pedestrian-friendly design and foster the civic associations of more public spaces, parking separating building frontages from the public street system shall be designed to have a “street feel,” incorporating pavement markings, street trees, street furniture and parallel parking.
 - G. In order to reduce the growth of surface parking which results in decreased densities, increased sprawl, separation of uses and anti-pedestrian environments the amount of parking shall be limited to no more than one hundred twenty-five percent (125%) of the required minimum amount.
 - H. Collector street(s) connecting the development to major arterials will be 22 to 24 feet in width with a raised or protected sidewalk (bollards, planting features) and a parking land on at least one side of the street.
 - I. Secondary streets connecting parking lots to main streets and collector streets should be 20 to 22 feet in width with a raised protected sidewalk.

11.4.4 PEDESTRIAN INFRASTRUCTURE GUIDELINES

- A. Pedestrian linkages shall be designed to provide the most direct route to the public transportation infrastructure to avoid out-of-direction travel and minimize travel distance. Linkages should be a continuation of the public infrastructure to reduce dead-end paths and encourage users to the development. Consideration should be given to off-site destinations in the design of the system.
- B. Pedestrian circulation routes shall be composed of treated surfaces such as scored, brushed, stamped and colored concrete, and brick pavers in order to differentiate the pedestrian system from the auto system. Where routes cross driveways a continuous raised crossing composed of a different paving material should be provided.
- C. Landscaping shall be provided between the pedestrian circulation system and automobile areas to provide protection, security and accessibility for the pedestrian. Parallel parking can also be used to buffer pedestrian routes from moving vehicles.
- D. Amenities such as benches, sitting areas, art, pedestrian squares and varied building facades shall be provided along pedestrian routes to add interest and convenience to the user.

-
- E. To provide for convenience, pedestrian facilities along building frontages and developments shall incorporate rain protection and boulevard landscaping whenever possible. When such facilities are provided, special attention should be given to ensure pedestrian safety, security and convenience by not creating enclosed spaces that may shelter potential criminal activity. Appropriate lighting shall be incorporated whenever possible.

Glossary

Bollard: A bollard is a vertical linear barrier or pole used to separate pedestrian from vehicles. A bollard can be approximately three feet in height, six to twelve inches in diameter, and connected by chains or other horizontal restraints.

Built Form: Built form is the resultant effect of previous construction in downtown Silverdale. The built form is the basis or foundation upon which all new development can occur. It constitutes the existing context or physical pattern that new development must consider.

Campus-Style: Campus-style refers to the grounds or fields that affect the organization and arrangement of buildings on a college campus. Formal variations of the “grounds” include quadrangles, commons, and other geometries.

Centers, Town or Village: Town or village centers are a shopping center hybrid that combine retail, office and residential uses with an open space square or green, and, in some cases, civic or semi-public uses such as libraries and community centers. A “main street” highlights the centers with narrow traffic lanes, on-street parking at least on one side, pedestrian walkways and building frontages close to the sidewalks. Referred to as “leisure centers,” they are programmed as gathering and meeting places in conjunction with retail and office uses. Residential uses are considered the necessary “glue” for successful Town Centers.

Composition(al) Structure: Composition is an aesthetically unified agreement of parts, i.e., the makeup of physical elements into an integrated settlement. As in art, a composition is how various parts or shapes are brought together to tell a “story.” In urban design, a composition is an arrangement of the individual buildings, streets and open space into a workable and agreeable whole. Developers can design according to their programmatic needs with flexibility and still fulfill the compositional relationships between building location, open space and street frontage.

Connectivity (Opportunities): Connectivity refers to physical design features that can join, couple or relate one part of downtown to another. For example, a private open space as a part of a mixed-use residential development along Clear Creek can be oriented to the creek and therefore “connect” the development to the creek corridor via open space features.

Context: Context refers to the parts, objects, physical patterns immediately adjacent or surrounding the center of attention. For example, the Clear Creek corridor is a contextual reference or setting for development parcels adjacent and/or facing the creek. The small scale block grid street and residential buildings are the context or setting for new development within the blocks. Context provides background identity and information for new projects that are inserted into that context.

Elements: Elements are basic physical substances or components of urban form, i.e., slopes, creeks, trees, streets, buildings, etc.

Framework: A framework is a basic structure or arrangement that holds objects (buildings, open space, etc.) together in a *reasonable* relationship. A block grid street network is a framework that determines the arrangement of parcels and, in turn, buildings within the blocks, forming an underlying foundation or frame for urban form. A super block street network with large setback requirements for buildings related to the street is a framework that determines the arrangement of buildings, parking and open space.

In-fill (Development): In-fill refers to new or reconfigured development occurring within a previously developed area. For example, two houses in a block of ten may be removed and replaced with four townhouses. The townhouses are defined as “in-fill,” filling the space left by the removal of the two houses. The in-fill, in turn, has a design response (responsibility) to the existing and remaining eight houses.

Intent, Design: A firmly directed or strongly resolved purpose, meaning or aim (of development and design).

Live/work Units: A structure or portion of a structure that combines an allowed commercial or manufacturing activity with a residential living space for the owner of the business/activity, where the owner is responsible for the business/activity performed. It differs from “home occupation” as the uses are considered equal instead of the business use as incidental to the residential use.

Local Street Network: A local street network is a loose grid of continuous and connected slow moving streets within the interior of the super blocks. They are a combination of public and private jurisdictions acting in consort to distribute traffic into and through new development. A series of “main streets” is incorporated into the network. The street network is accompanied by a continuous pedestrian walkway network.

Mixed-Density: Mixed-density development incorporates a variety of uses and building types, cluster or development component. For example, a mixed-density mixed-use development can have residential or office over retail or separated building structures plus townhouses, garden flats and courtyard housing all integrated into one development. The purpose is to provide a variety and diversity of building design and scale while attracting a variety of residents and consumers.

Mixed Use Centers: Mixed Use centers are similar to Town Centers in their incorporation of a variety of retail, office and residential uses. Civic and cultural uses are not a key part of a mixed-use center. Open space squares and greens are a part of the program along with a “main street” approach.

Orientation, Building: Orientation refers to the adjustment or adaptation of a building or artifact to a particular situation such as a village square, sidewalk or compass direction.

Pedestrian Network: A hierarchy of connected and continuous pedestrian promenades, concourses, walkways, paths and trails forming a slow moving mobility network throughout downtown.

Pedestrian-Oriented: Pedestrian-oriented refers to a safety, comfort, convenience environment for the citizen who walks through downtown. *Safety* can consist of a clear separation between walking area and vehicular lane. *Comfort* can consist of weather protection devices to shield citizens from rain, wind and other climactic situations. *Convenience* can consist of places to rest, sit, view interesting features and talk with friends without excessive noise, pollution and other distractions.

Principle, Design: A rule of conduct, a fundamental rule upon which other actions are based. A set of rules that explains a method of operation for the redevelopment of downtown Silverdale.

Quadrangles: An area surrounded on two to four sides by buildings, as in a campus plan.

Super Block: A super block is a large tract of land that is served primarily by a few peripheral arterials. Local access streets are few, if any, and consist of driveways and parking lot lanes. In Silverdale, super blocks are characterized by low intensity activities supported by large surface parking lots and multiple access drives to the arterials.

Types, Building: Types are the various plan and form characteristics associated with a particular class or group of buildings. For example, a townhouse is different from a single family detached building. A townhouse is a series of individual units attached at common side walls with at least two stories for each dwelling unit. A double-loaded corridor residential building (condominium or rental) is a building with a linear interior access corridor that serves units on both sides of the corridor. Each type has appropriate applications regarding site and in-fill development requirements.

Urban Design: Urban design is the study of the organization and structure of cities and settlements. Urban design focuses on the ways and means of coordinating and integrating bio-physical, cultural and jurisdictional factors of settlements into a working and aesthetic whole. There are many interpretations of urban design: for this handbook, urban design is viewed as a decision-making process that assists a community in restructuring or reassembling the land, streets, buildings, building orientation and open space into a specific desirable outcome, benefiting both individuals and the community-at-large. Urban beautification is a subset of urban design, dealing with the surficial aspects of design.

Urban Form: Urban form is the physical arrangement or structure of a city or settlement. Form is expressed in the larger patterns of land, streets, parcel size and building type.